

Oakmont Village & Eagle Road Corridor: Goals, Objectives & Strategies

Haverford Township - Delaware County, Pennsylvania

Overarching Goals: To Transform these Commercial Districts into a more attractive, functional, and enjoyable Streetscape environment. To promote a sustainable and integrated public realm for civic, retail, service, office, and residential uses, by employing the objectives and strategies set forth below.



Locate Buildings to help Define the Public Realm:

- Position buildings close to sidewalks
- Group buildings together
- Anchor corners with buildings

Create a more Pedestrian - Oriented Place:

- Promote walkable shopping experiences
- Provide wide sidewalks
- Promote attractive storefronts & window displays
- Enhance facades
- Create a marketable identity

Provide for improved Pedestrian Accessibility:

- Maintain and expand sidewalk networks
- Widen sidewalks
- Provide crosswalks
- Create a pedestrian promenade in front of stores and shops
- Institute traffic calming measures

Create and Maintain Gateways & Streetscape Character:

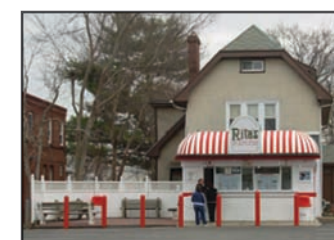
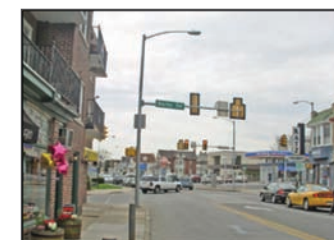
- Create Gateways in Oakmont, at Lawrence Road, and at edges of the study area (see map)
- Provide vertical banners along the streetscape
- Maintain and create an "outdoor room" character with a well-designed Streetscape

Provide for Mixed Use:

- Maintain two-story buildings
- Encourage second floor use for service businesses and offices
- Promote apartments above commercial
- Diversify land uses

Provide for Alleys, Service Lanes, and Service Drives:

- Maintain existing alleys & service lanes
- Create new alleys & service lanes
- Create opportunities for shared service drives
- Minimize curb cuts



Better Manage Off-Street Parking Lots:

- Locate parking to serve multiple-uses/lots
- Promote shared parking & shared parking lots
- Buffer off-street parking lots with hedges and other landscaping
- Position off-street parking lots to the rear and side of buildings where feasible

Provide for On-Street Parking:

- Maintain existing on-street parking
- Provide new on-street parking where feasible
- Allow on-street parking in front of shops and stores to count towards required parking
- Minimize / reduce curb cuts

Integrate Building / Sidewalk Relationships:

- Maintain buildings in alignment
- Position sidewalks parallel to building frontages
- Create a pedestrian promenade, the "Haverford Walk" as shown in the Development Strategy Plan

Provide for Building Transitions:

- Maintain porches
- Provide porticos
- Provide stoops
- Provide colonnades
- Provide pergolas

Provide and Maintain Street Trees, "Street Furniture", and other Landscaping:

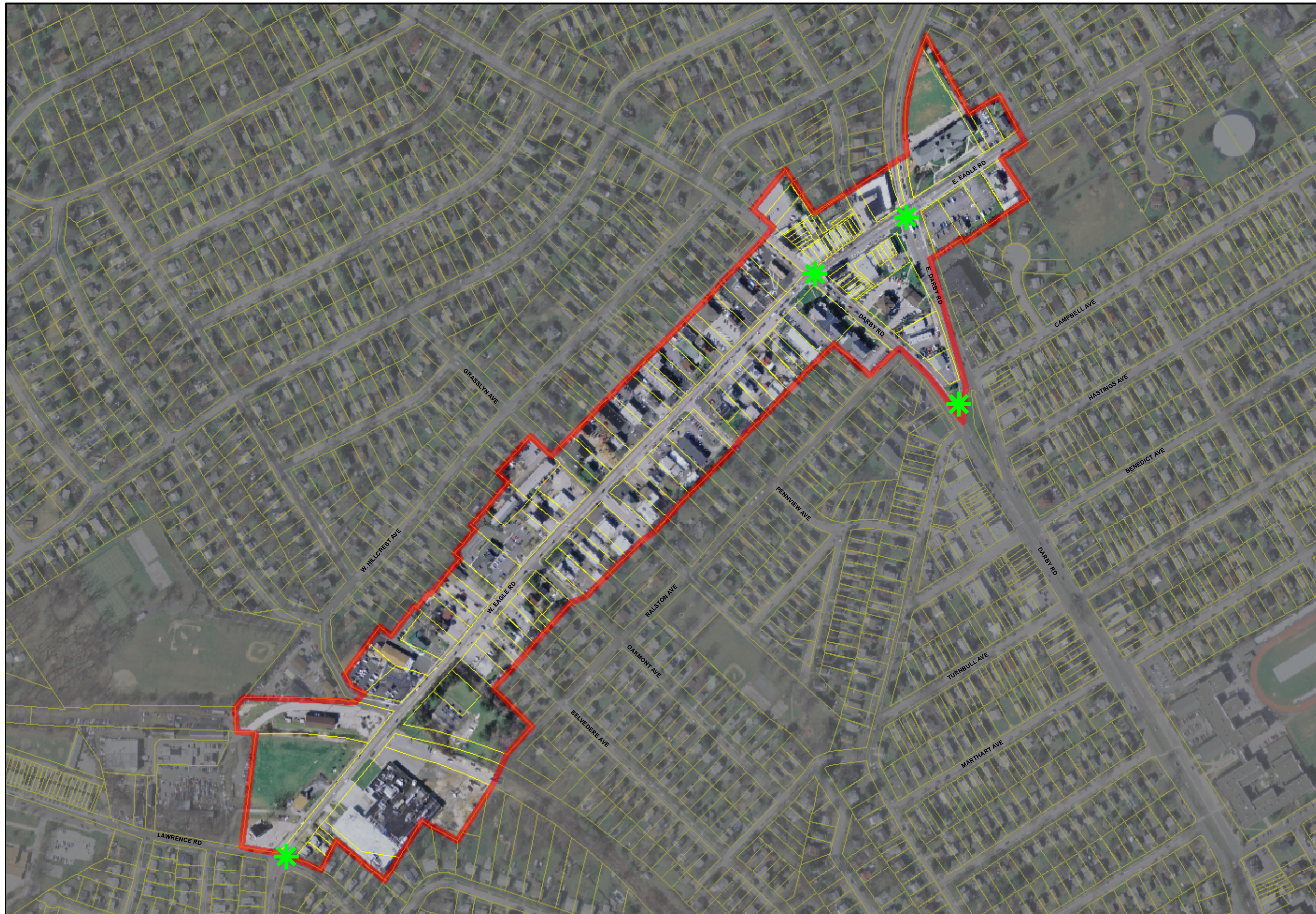
- Provide street trees and hedges
- Provide planters
- Provide street lights
- Provide benches
- Maintain plazas and recreational areas
- Provide passive recreational areas

Accomodate Bicycling:

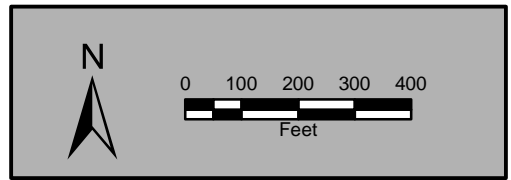
- Permit bicycling
- Provide bicycle racks
- Provide crosswalks

Improve the Quality of Signage:

- Minimize free-standing pylon signs
- Promote ground signs and wall signs
- Promote well-designed signs



Oakmont Village & Eagle Road Corridor
 Comprehensive Plan Addendum: Visioning Component
 Haverford Township
 Delaware County, Pennsylvania



Data Source:
 Base Features & 2005 Aerial
 Photos: Delaware County Planning
 Department, May 2007

Legend

- Oakmont Village & Eagle Road Study Area
- * Gateways
- Parcels

Thomas Comitta Associates, Inc.
 Town Planners & Landscape Architects
 18 West Chestnut Street
 West Chester, PA 19380
 June 14, 2007; Revised: June 22, 2007
 June 29, 2007; October 12, 2007

Design Guidelines:

Oakmont Village & Eagle Road Corridor

Haverford Township, Delaware County, PA



Comprehensive Plan Addendum

Prepared by:
Thomas Comitta Associates, Inc.
Town Planners & Landscape Architects

September 27, 2007; October 18, 2007
November 20, 2007; December 12, 2007
January 4, 2008



Design Guidelines: Oakmont Village & Eagle Road Corridor

Haverford Township - Delaware County, PA

Outline of Contents:

September 27, 2007; October 18, 2007; November 20, 2007; December 12, 2007; **January 4, 2008**

1. Purpose & Intent
2. Streetscape & Street Walls
3. Building Location
4. Infill Buildings
5. Service Lanes & Alleys
6. Off-Street Parking
7. Curbs & Curb Cuts
8. Mixed Use(s)
9. On-Street Parking
10. Sidewalks
11. Crosswalks
12. Pedestrian Promenade
13. Pedestrian Gathering Places / **Pedestrian Pockets**
14. Building Size, Height & Design
15. Storefronts & Window Displays
16. Porches, Porticos & Stoops
17. Street Trees
18. Hedges, Fences & Walls
19. Street Lights
20. Street Furniture
21. Traffic Calming
22. Signage
23. Gateways
24. Wayfinding Signage
25. Bicycle Opportunities
26. Accessory Structures
27. Stormwater Management

Purpose & Intent

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Buildings Located Close to Sidewalks in Oakmont Village and Setback along Eagle Road



Buildings Grouped Close Together, Oakmont Village

Legislative Intent:

- 1.1 This Manual is intended to comply with Section 708-A of the Pennsylvania Municipalities Planning Code titled: Manual of Written and Graphic Design Guidelines.
- 1.2 Placemaking, as described and shown in this Manual, is intended to create a more functional and attractive outcome for the quality of life in Haverford Township in general, and in Oakmont Village and along Eagle Road in particular.

Design Guidelines:

- 1.3 This Manual shall be utilized to plan, design, construct and maintain buildings, structures, streetscapes, landscapes and hardscapes.
- 1.4 The places, spaces, buildings, streetscapes, etc. shown and described as “Best Practices” in this Manual shall be emulated.
- 1.5 All land development and redevelopment plan submissions shall be accompanied by Architectural Plans and Building Elevations that are consistent with this Manual, and the Development Strategy Plans.



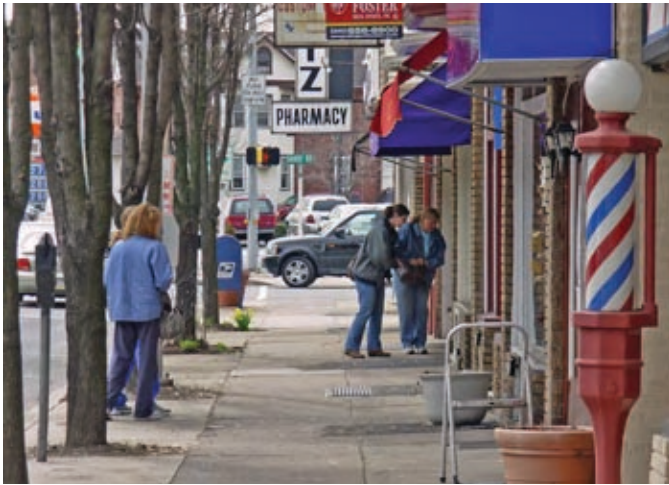
Best Practices example of Street Trees, Media, PA



Street Trees and Buildings Located Close to Sidewalks in Oakmont Village

Streetscape & Street Walls

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Buildings Adjoining Sidewalk Promote Pedestrian Activity of the Streetscape



Shopfronts in Oakmont Village Enhance the Streetscape Environment

Legislative Intent:

- 2.1 The Streetscape is intended to be formed by buildings located close to the sidewalk, and/or to be enhanced with such features as street trees, street lights, sidewalks, stoops, and like type amenities.
- 2.2 Street walls are intended to help form and frame the streetscape and the traditional village character for Oakmont and Eagle Road.
- 2.3 Street walls are intended to buffer parking.

Design Guidelines:

- 2.4 The Streetscape shall be embellished with street trees, street lights, sidewalks, fences porches, stoops and other front yard amenities in front of buildings.
- 2.5 Buildings in alignment shall be maintained and/or provided to form Street Walls.
- 2.6 Whenever buildings can not form the Street Wall, the options shown on pages 2.2 through 2.5 and in the photo below, shall be utilized.



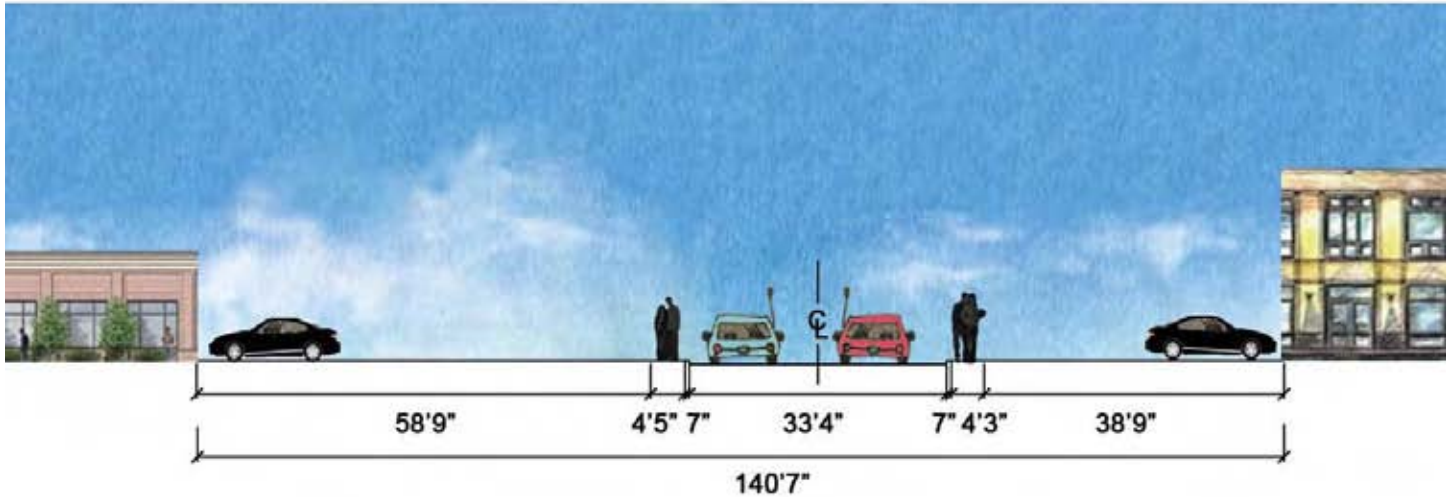
Street Wall Located Close to Sidewalk Screens Off-Street Parking along Eagle Road



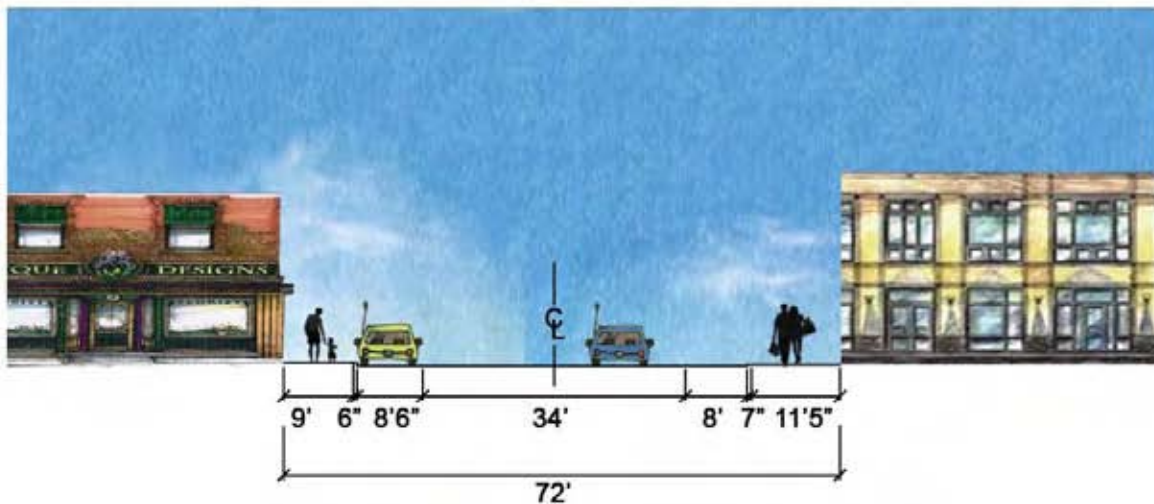
Best Practices example of Street Wall at Corner Parking Lot, West Chester, PA

Streetscape & Street Walls

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Typical Eagle Road Street Section

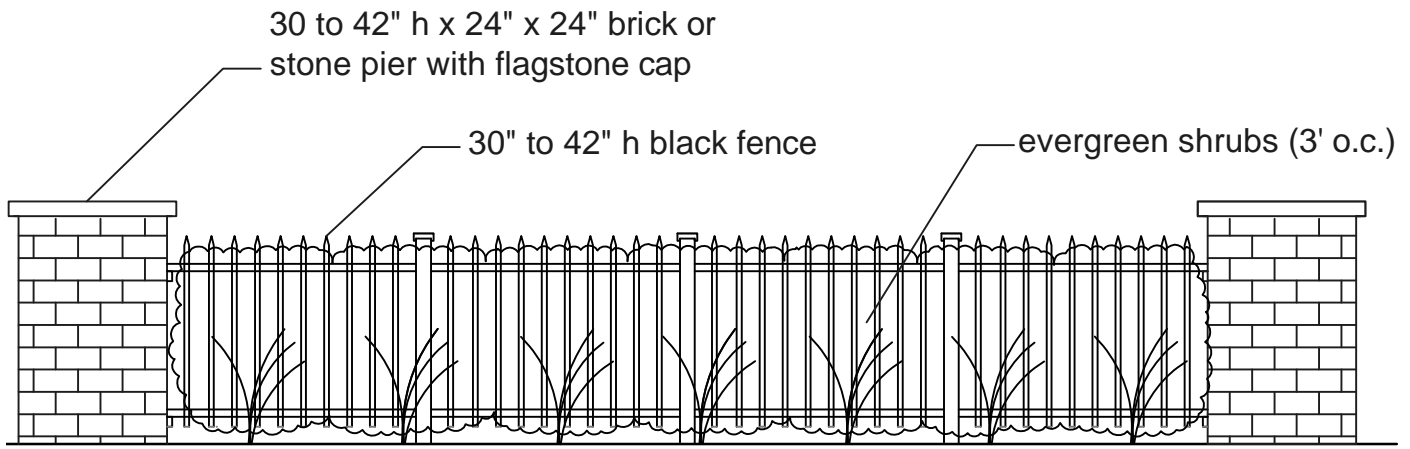


“Cozy” Oakmont Village Street Section

Note: Whenever the Street Wall to Street Wall dimension is in the 70 to 85 foot range, a more Village type scale and “Main Street” environment exists.

Streetscape & Street Walls

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



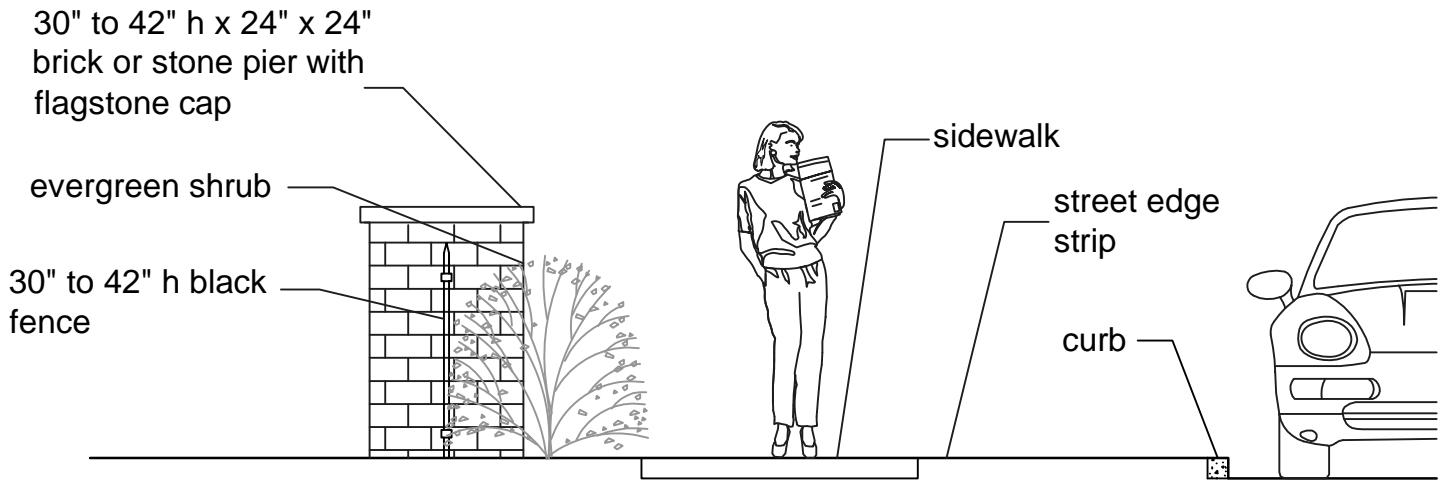
TYPICAL SECTION OF PIER, FENCE,
AND HEDGE



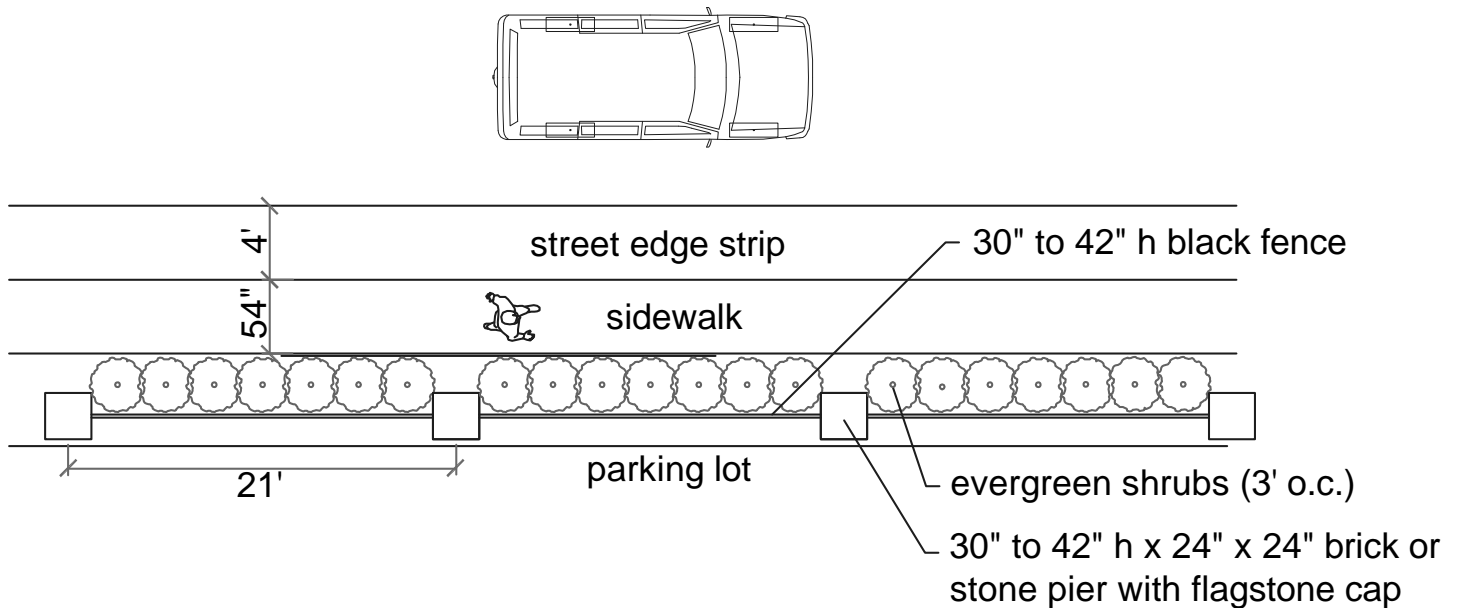
BRICK PIER, FENCE, AND HEDGE AS PARKING LOT
SCREEN

Streetscape & Street Walls

Design Guidelines: Oakmont Village & Eagle Road Corridor
 Haverford Township, Delaware County, PA



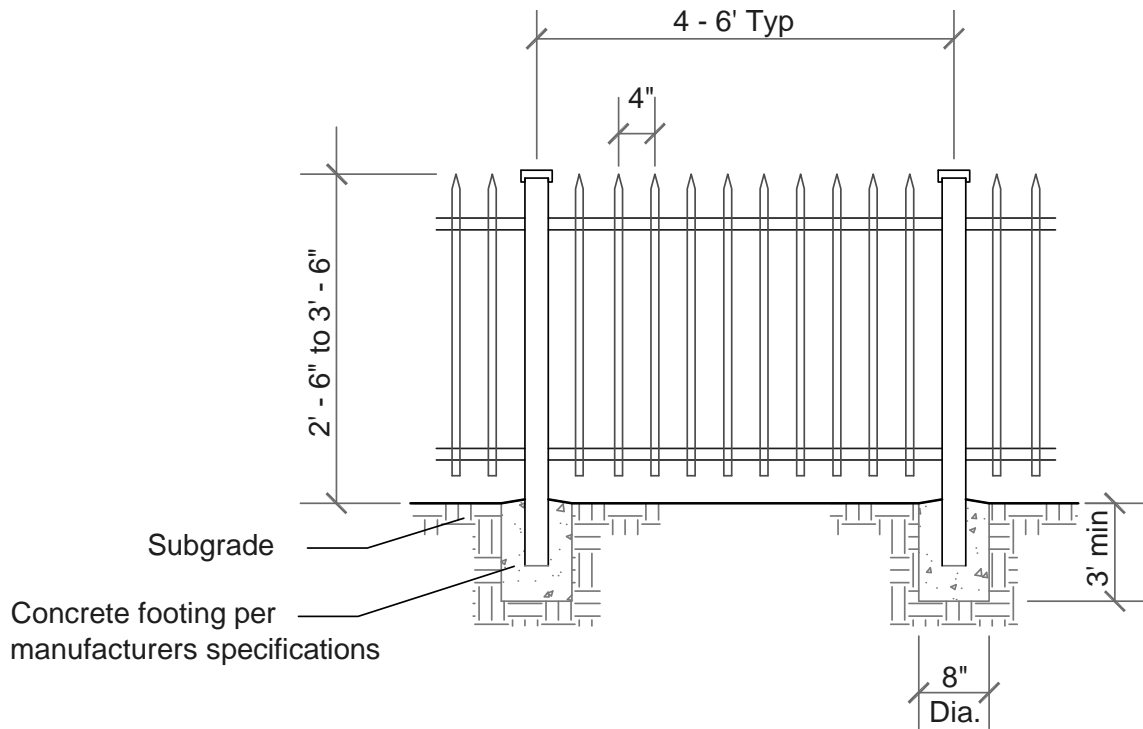
TYPICAL ELEVATION OF PIER, FENCE, AND HEDGE



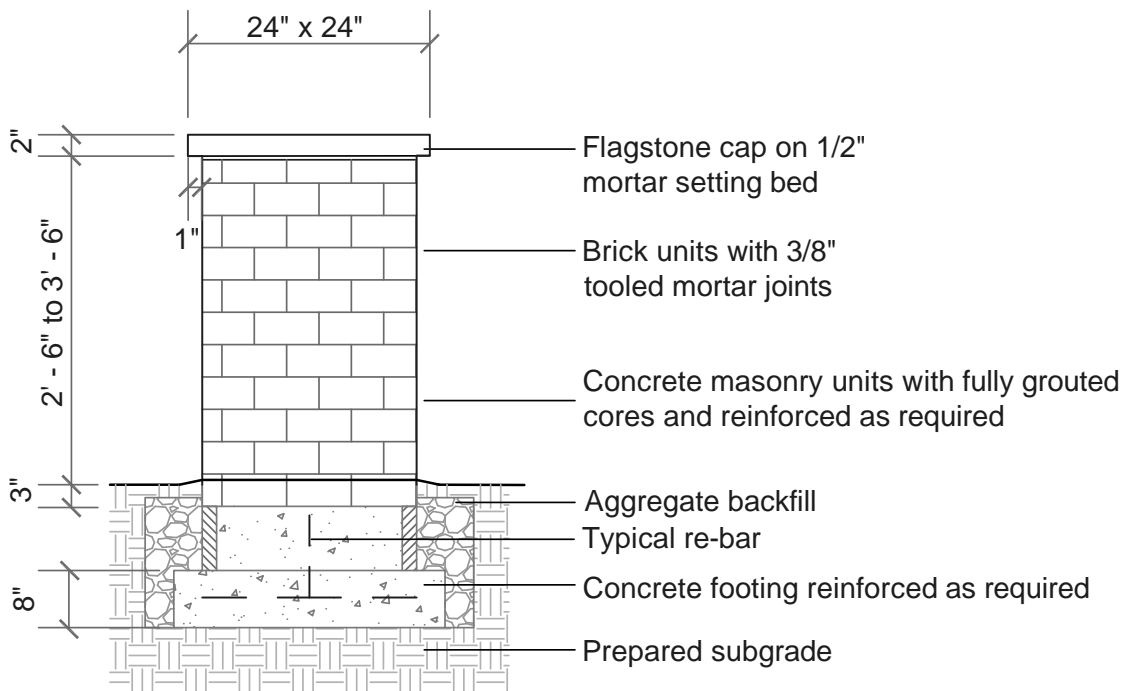
TYPICAL PLAN VIEW OF PIER, FENCE, AND HEDGE

Streetscape & Street Walls

Design Guidelines: Oakmont Village & Eagle Road Corridor
 Haverford Township, Delaware County, PA



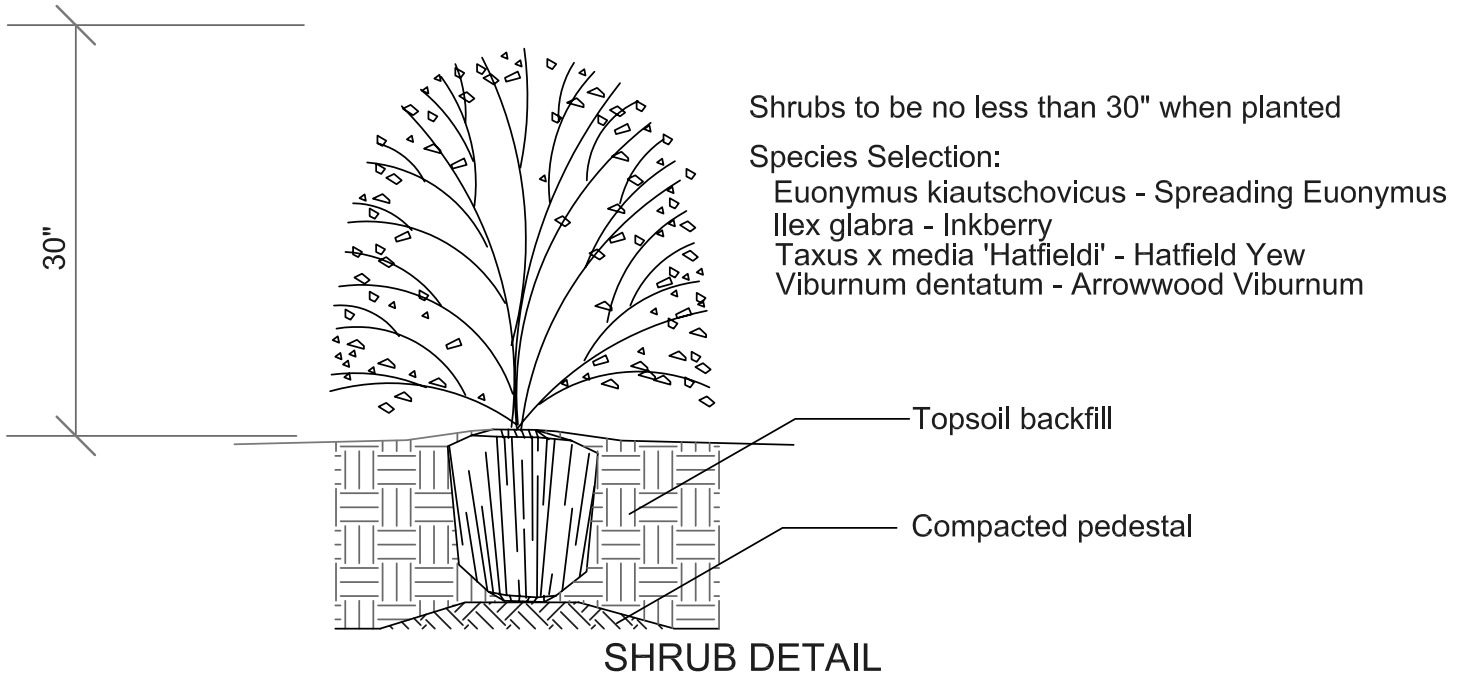
FENCE DETAIL



PIER DETAIL

Streetscape & Street Walls

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Building Location

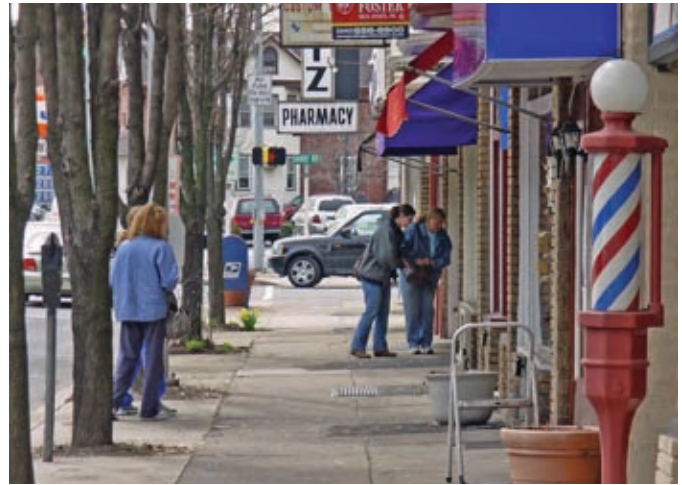
Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Buildings Located Close to Sidewalk Help to Create the “Outdoor Room” character in Oakmont Village

Legislative Intent:

- 3.1 Buildings are intended to be located close to the street to help form the Streetscape.
- 3.2 New buildings are intended to be built and maintained at a “Build-To” Line, in alignment with existing traditional buildings on a block.



Buildings Forming the Build-To Line in Oakmont Village

Design Guidelines:

- 3.3 New buildings shall be located in line with existing traditional buildings on a block.
- 3.4 New buildings shall be located at a Build-To line measured from the edge of the right-of-way of Eagle Road, or within four (4) feet of the average setback of existing buildings on a block.
- 3.5 If a building cannot be located at a Build-To line, a street wall shall be provided as set forth in the previous Section 2.



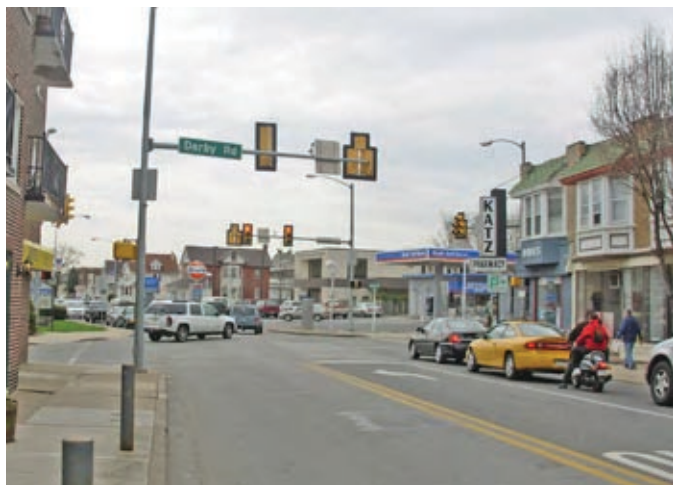
Buildings Forming the Build-To Line



Buildings Help to Create the “Outdoor Room” Character of Oakmont Village

Infill Buildings

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Opportunities for Infill Buildings around South of Oakmont Village



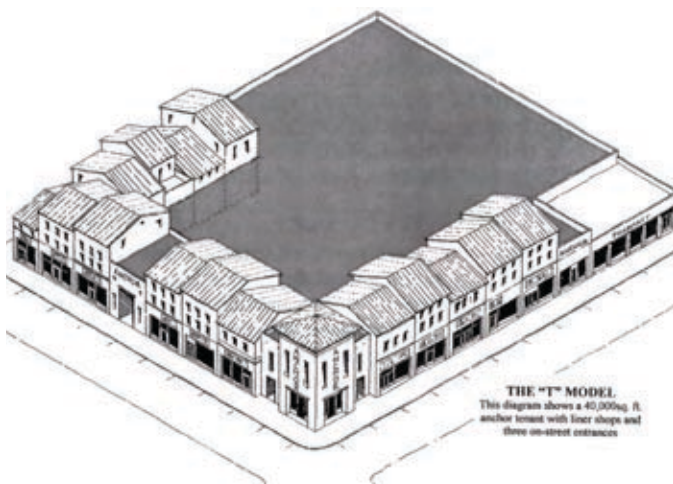
Buildings Anchor Street Corners

Legislative Intent:

- 4.1 Infill development is intended to be consistent with the existing traditional buildings on the block, in terms of adjacency or proximity of buildings to one another and to the sidewalk.

Design Guidelines:

- 4.2 Locate new buildings at a Build-To line, as described in the previous Section 3.
- 4.3 “Anchor” street corners with buildings wherever possible..
- 4.4 Size new buildings in proportion with traditional buildings in Oakmont Village and along Eagle Road.
- 4.5 If a larger footprint building is proposed, it should be “edged” with Liner shops.



Larger Footprint Building or Deck Parking Garage as a Possible Infill Opportunity “edged” by Liner Shops



Existing Traditional Shops

Infill Buildings

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



New Two-Story Sovereign Bank, Lantern Hill, Doylestown, PA



CVS Drugstore and Streetscape, Eagle, PA

Legislative Intent:

- 4.6 New buildings are intended to complement existing two-story buildings.
- 4.7 New buildings are intended to promote a village scale and character.

Design Guidelines:

- 4.9 Maintain existing two-story buildings to enhance mixed-use opportunities.
- 4.8 Build infill buildings as two-story structures, in context with a village scale environment.



New Two-Story Dunkin' Donuts, Claymont, DE



Gas Station / Convenience Store, Meredith, NH

Service Lanes & Alleys

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



New Alley in Gettysburg, PA is example of Best Practice from a redevelopment initiative



Narrow Curb Cut for Alley in Oakmont Village

Legislative Intent:

- 5.1 Parking accessed from service lanes and alleys is intended to eliminate conventional curb cuts along street frontage, and promote curb appeal along the lot frontage.
- 5.2 Service lanes and alleys are intended to provide opportunities for the installation of utilities, and for servicing a lot from the rear.

Design Guidelines:

- 5.3 Service lane and alley widths shall be at least 16 feet in width for two-way travel, and 12 feet for one-way travel.
- 5.4 Curb cuts for alleys shall be limited to 24 feet in width.
- 5.5 Alleys shall be landscaped with trees, shrubs, groundcovers, fences, and other features, to add charm and beauty.
- 5.6 The driveway apron in front of a garage off an alley shall be at least 8 feet in width to allow a vehicle to park parallel to the alley, or 18 feet in length for perpendicular parking.



Frontage Curb Appeal Due to Alley in Rear of Building in Oakmont Village



Alleys Provide Opportunities for Servicing a Lot from the rear in Oakmont Village

Off-Street Parking

Design Guidelines: Oakmont Village & Eagle Road Corridor
 Haverford Township, Delaware County, PA



Off-Street Parking Screened with Wall and Trees along Eagle Road



Example of Best Practice for Street Wall of new Wawa in Claymont, Delaware, Buffering Off-street Parking Lot.

Legislative Intent:

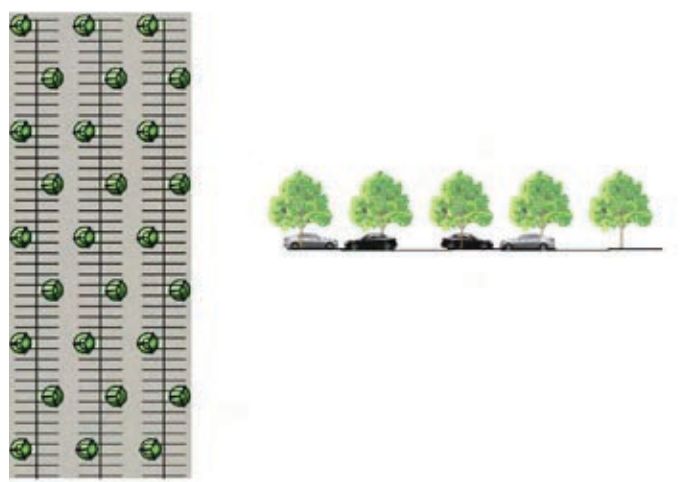
- 6.1 Buildings are intended to screen the parking behind them.
- 6.2 Existing parking lots that are located in front of buildings are intended to be screened with landscaping and/or walls.
- 6.3 Off-street parking lots are intended to be generously landscaped.

Design Guidelines:

- 6.4 New parking shall be located to the rear or side of buildings to the maximum extent possible.
- 6.5 Off-street parking visible from a street shall be screened with walls, fences, hedges and other landscaping at a minimum of 30 inches in height.
- 6.6 Parking lot islands and peninsulas shall be located and landscaped to “break-up” expanses of asphalt, whereby no more than fifteen (15) parking spaces shall be in a row (preferably no more than ten (10) in a row) without being interrupted by a landscaped island.



Best Practice example of Parking Lot with Generous Landscaping, Exton, PA



Best Practice example of Shade Trees in an Off-Street Parking Lot

Curbs & Curb Cuts

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Continuous Curb Without Curb Cut in Oakmont



Narrow Curb Cut Minimizes Pedestrian Conflicts in Oakmont Village

Legislative Intent:

- 7.1 Curb cuts are intended to be limited in width, to help control vehicular access and increase safety for pedestrians.
- 7.2 Existing wide curb cuts are intended to be “necked-down” to minimize pedestrian conflicts and to provide space for additional landscaping.

Design Guidelines:

- 7.3 New mid-block curb cuts shall not be created.
- 7.4 Excessively wide curb cuts shall be narrowed to 24 feet where possible.
- 7.5 Existing cross streets and service drives and alleys along the rear of the lot shall be utilized to access parking to the maximum extent possible.
- 7.6 Concrete curbs shall be installed and maintained in accordance with the Haverford Township Subdivision & Land Development Ordinance.



Wide Curb Cuts Should be Narrowed to Provide Increased Safety and Pedestrian Orientation along Eagle Road



Wide Curb Cut Along Eagle Road Increases Vehicular and Pedestrian Conflicts

Mixed Uses

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Apartment Above Water Ice Stand, Eagle Road



Apartments Above Commercial along Eagle Road

Legislative Intent:

- 8.1 New Uses are intended be mixed horizontally or vertically.
- 8.2 Live-Work Units are intended to be exceptionally good vertical mixed use buildings.
- 8.3 Mixed Use buildings are intended to provide opportunities for more affordable housing.

Design Guidelines:

- 8.4 Horizontal and vertical mixed-use buildings should be built to the maximum extent possible.
- 8.5 Live-Work Units should be provided to the maximum extent possible.
- 8.6 Offices and/or apartments should also be considered above first floor retail where feasible.



Best Practices example of Ground Floor Cafe, with Apartments Above, Gainesville, FL



Best Practices example of Live-Work Units at Eagleview, Exton, PA

On-Street Parking

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Best Practice example of On-Street Parking along State Street, Media, PA



On-Street Parking Provides a Buffer for Pedestrians in Oakmont Village

Legislative Intent:

- 9.1 On-street parking is intended to provide vital overflow parking for patrons and guests.
- 9.2 On-street parking is intended to provide a useful form of traffic calming.
- 9.3 On-Street Parking is intended to provide an “insulation” from vehicular traffic for pedestrians along sidewalks.

Design Guidelines:

- 9.4 On-street parking shall be provided along new streets where feasible.
- 9.5 On-street parking bays shall be 7 feet wide by 22 feet long.
- 9.6 On-street parking shall be utilized as guest parking and overflow parking for guests and patrons.



On-Street Parking is a form of Traffic Calming in Oakmont Village



On-Street Parking in Oakmont Village Insulates Pedestrians from Vehicular Traffic

Sidewalks

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Pedestrian Friendly Sidewalks Promote Walkability in Oakmont Village



Window Shopping Enhanced by Sidewalks in Oakmont Village

Legislative Intent:

- 10.1 Sidewalks are intended to provide opportunities for continuous pedestrian circulation.
- 10.2 Sidewalks are intended to add a pedestrian friendly feature to the Streetscape.

Design Guidelines:

- 10.2 Sidewalks shall be maintained along both sides of all streets.
- 10.3 All new sidewalks shall be at least 4' - 6' in width (preferably 5'0" in width) and shall be connected to the buildings on a lot.
- 10.5 All sidewalks shall be constructed of either brick, poured in place concrete, or other material approved by Haverford Township.



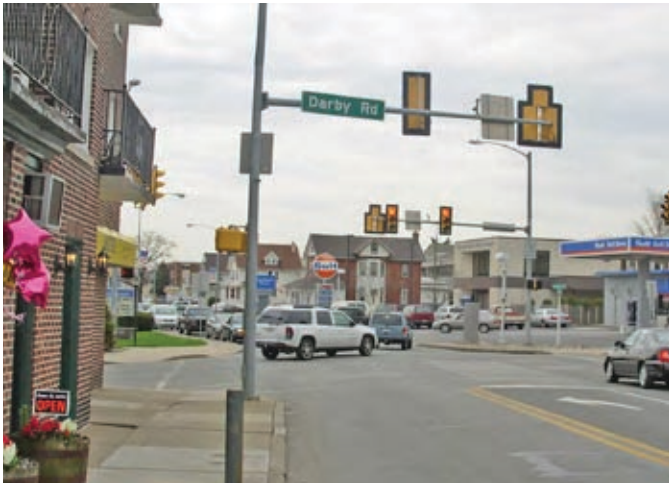
Sidewalks with Small Curb Cuts Reduce Pedestrian and Vehicular Conflicts in Media, PA



Sidewalks with Wide Curb Cuts Diminish Pedestrian Presence

Crosswalks

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Well-defined Crosswalks are needed to Continue the Sidewalk System



Best Practices example of Crosswalk, State Street, Media, PA

Legislative Intent:

- 11.1 Crosswalks are intended to continue the sidewalk system in the Township.
- 11.2 Crosswalks in driveway throats (at curb cuts) are also intended to continue the sidewalk system.

Design Guidelines:

- 11.3 Crosswalks shall be at least 6'-0" in width when they cross streets.
- 11.4 Crosswalks located in driveway throats shall be at least 5'-0" in width.
- 11.5 Crosswalks shall be unit pavers such as E.P. Henry Mediterranean interlocking pavers or approved equal.
- 11.6 Where unit pavers can not be installed, zebra stripes shall be painted and maintained.

(continued on page 11.1)



Best Practice example of Brick Crosswalk in Driveway Throat, West Chester, PA



Well-Defined Zebra-Stripe Crosswalks at Street Intersection, Easton Town Center, OH

Crosswalks

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Best Practices example of an At-Grade Crosswalk, West Chester, PA



Mid-Block Crosswalk, Ephrata, PA

Legislative Intent:

11.7 Where possible, crosswalks at street intersections should be at grade to enhance pedestrian circulation.

Design Guidelines:

11.8 Where approved by Penn Dot, mid-block crosswalks should be signed with pedestrian safety in mind.



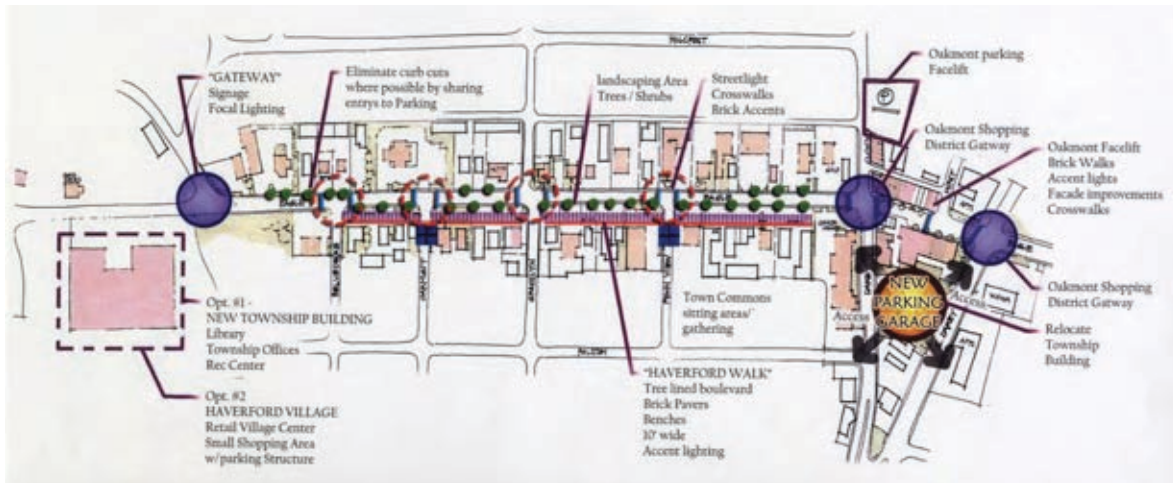
Zebra-Stripe Crosswalk



Zebra-Stripe Crosswalk, Philadelphia, PA

Pedestrian Promenade

Design Guidelines: Oakmont Village & Eagle Road Corridor
 Haverford Township, Delaware County, PA



Conceptual Plan for "Haverford Walk", December 2006

Legislative Intent:

12.1 A Pedestrian Promenade is intended to enhance the Eagle Road Corridor for pedestrian use and enjoyment, in conjunction with the redesign of Eagle Road as a boulevard-type street.

Design Guidelines:

- 12.2 New development, redevelopment, and infill development should be generally consistent with the Conceptual Plan shown above.
- 12.3 A Pedestrian Promenade at least ten (10) feet in width (preferably 12 feet in width) should be provided and maintained in front of buildings along Eagle Road, in accordance with the Development Strategy Plan.

continued on page 12.1



Pedestrian Promenade at Haile Village Center



Pedestrian Promenade at Main Street at Exton, Exton, PA

Pedestrian Promenade

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Boulevard Street, London, U.K.



Pedestrian Promenade and Angled Parking Ardmore, PA

Design Guidelines:

- 12.4 Portions of Eagle Road should be transformed into a more boulevard-type street in order to better accommodate the Pedestrian Promenade and to create a more attractive Streetscape.
- 12.5 Angled parking adjoining the pedestrian promenade should be considered to economize on parking spaces.

Design Guidelines:

- 12.6 The boulevard* should be consistent with Development Strategy Plan 'E', dated February 8, 2008.

* (see proposed Boulevard cross-section on page 12.2 for complete details)



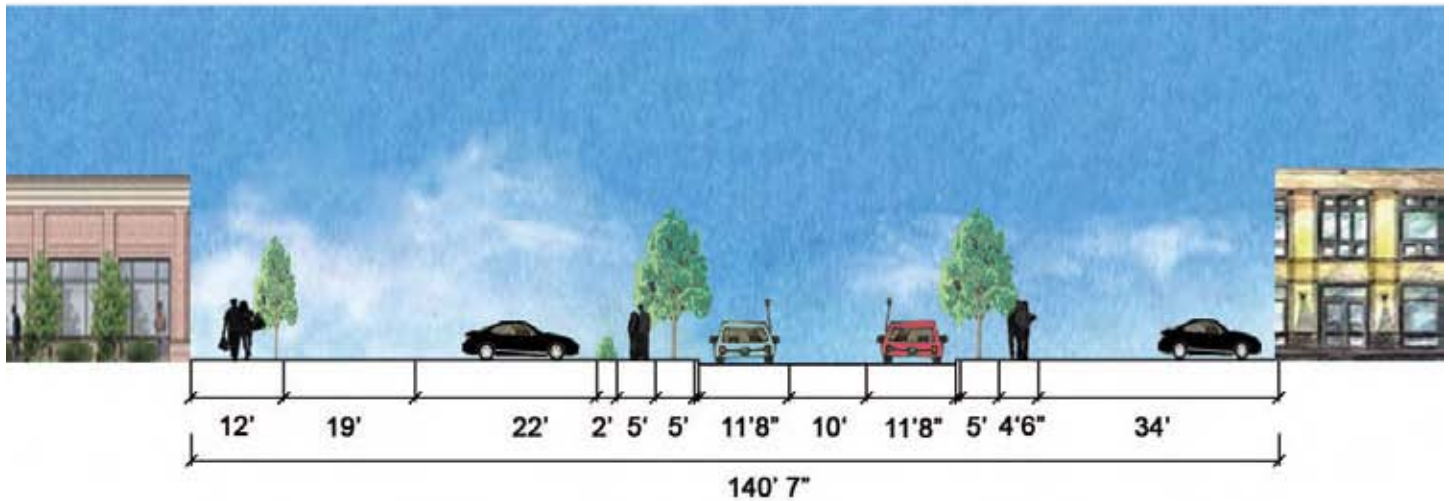
Boulevard Street, Holland, MI



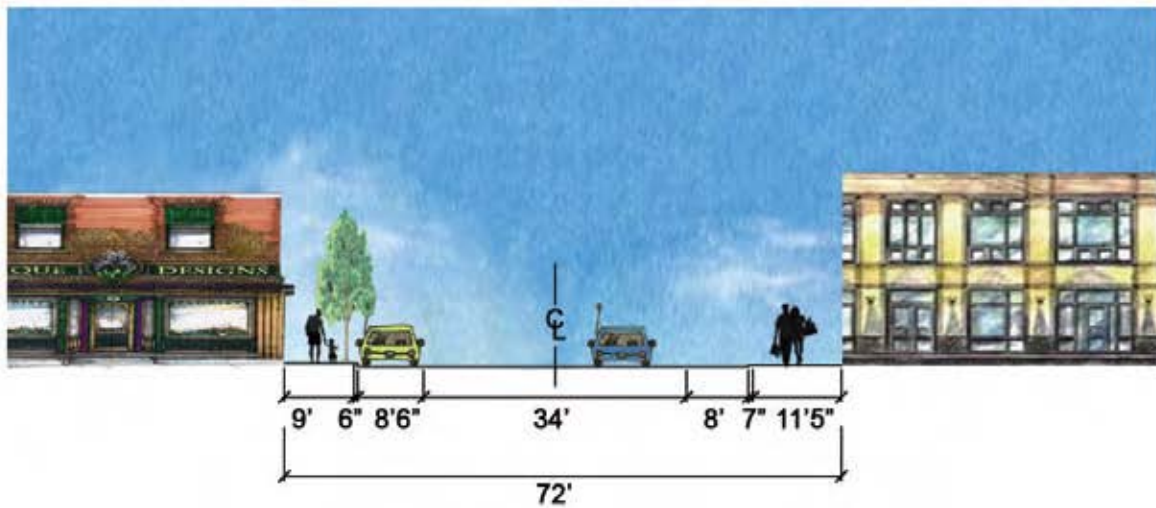
Boulevard Street, Holland, MI

Pedestrian Promenade

Design Guidelines: Oakmont Village & Eagle Road Corridor
 Haverford Township, Delaware County, PA



“Eagle Boulevard” Street Section



“Cozy” Oakmont Village Street Section

Notes:

- 1.1 See Development Strategy Plan ‘E’ for Eagle Boulevard concept.
- 1.2 Whenever the Street Wall to Street Wall dimension is in the 70 to 85 foot range, a more Village type scale and “Main Street” environment exists.

Pedestrian Gathering Places / Pedestrian Pockets

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Siting Wall, Newport, RI



Civic Plaza, Cambridge, MA

Legislative Intent:

13.1 Pedestrian gathering places and “pedestrian pockets” are intended to provide viable opportunities to celebrate the public realm.

Design Guidelines:

13.2 Construct and maintain plazas, pavilions, gazebos, and sitting areas as viable pedestrian pockets.

13.3 Install and maintain benches, sitting walls, shade trees, plantings, and lighting in pedestrian areas to enhance attractiveness and safety.



Sitting Area, State College, PA



Civic Plaza, Sewickley, PA

Building Size, Height & Design

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Best Practices example of Ground Floor Commercial Liner Shops, Bicentennial Garage, West Chester, PA



Two-Story Buildings with First Floor Retail in Graceful, Narrow-Bay Sequence in Oakmont Village

Legislative Intent:

- 14.1 The use of traditional materials, such as brick, stone, stucco over stone, and wood siding, is intended to provide a historic reference to the Corridor.
- 14.2 Facade articulation, variation in roof lines, and vertical expression of buildings, is intended to promote consistency with the scale and proportion of traditional village streetscapes.
- 14.3 Two-story buildings are intended to promote a better scale and village-type environment.

Design Guidelines:

- 14.4 Brick, stone, stucco over stone, or wood siding shall be utilized to the maximum extent possible.
- 14.5 A primarily vertical expression to buildings shall be created through the use of crenelation in the form of windows, doors, pilasters, piers, columns, porches, porticos, stoops, colonnades, and the like.
- 14.6 Recesses or projections to buildings, from one to four feet, shall be provided whenever the building exceeds 24 feet in width.
- 14.7 Building heights shall be a minimum of two (2) stories, and a maximum of three (3) stories or 45 feet.



Vertical Expressions to Break Up the Horizontality



Best Practices example of Two-Story Bank, Media, PA

Building Size, Height & Design

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Two-Story Buildings with Vertical Bays, the Waterfront, Pittsburgh, PA

Design Guidelines:

- 14.8 Pilasters shall be extended to the sidewalk as a full vertical unit.
- 14.9 The roof lines of buildings shall be varied through the use of dormers, gables, turrets, and the like, to the maximum extent possible.
- 14.10 Facade wall heights of flat-roofed buildings shall be staggered to create visual interest. The heights of such buildings shall vary by at least 42 inches through the use of parapet walls.
- 14.11 Facades, roof lines, and building heights shall be provided and articulated along the primary facade of a building. On corner lots, all buildings shall have two primary facades, one for each street frontage.



Articulated Facade with Vertical Bays, and Articulated Roof, United Savings Bank, Media, PA

Design Guidelines:

- 14.12 Building design and materials shall emulate the character of buildings shown in the photographs on pages 14 and 14.1.
- 14.13 Architectural Plans and Building Elevations shall be submitted with all submissions for land development.
- 14.14 Minimum 2-story building height regulations are intended to promote less building coverage, and engender a more village-type scale.



New Infill Development with Effective Vertical Pilasters



Facade Articulation Enhances Corner Store Attractiveness at Southern Village, Chapel Hill, NC

Storefronts & Window Displays

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Best Practices example of Attractive Storefront, Sewickley, PA



Attractive Storefront and Window Display in Oakmont Village

Legislative Intent:

15.1 Attractive storefronts and window displays are intended to enhance the success of businesses.

Design Guidelines:

15.2 Attractive storefronts and window displays shall be created and maintained.

15.3 Awnings should be utilized to the maximum extent possible.

15.4 Window signs should be limited in size to enable better visibility of products for sale.



Best Practices example Of Storefronts at Bicentennial Garage, West Chester, PA



Best Practices example Of Storefronts Main Street at Exton, Exton, PA

Porches, Porticos & Stoops

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Porch Provides Shelter for Patrons Along Eagle Road



Best Practices example of Portico at Rolliers Hardware Store, Pittsburgh, PA

Legislative Intent:

16.1 The Porch, Portico, and Stoop is intended to promote a traditional village character, and to serve as a transitional element from the private realm of a building to the public realm of the streetscape.

Design Guidelines:

- 16.2 New development shall have porches, porticos, or stoops to the maximum extent possible.
- 16.3 Porches shall be at least 7'-0" deep and 10'-0" wide.
- 16.4 Porticos shall be at least 5'-0" deep and 6'-0" wide
- 16.5 Porches should not be enclosed.



Enclosed Porch Reduces Building Permeability Along Eagle Road



Best Practices Stoop example at Trader Joe's, Crocker Park, Ohio

Street Trees

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Effective Street Tree Spacing in Oakmont Village



Street Trees Help to Define Public Realm in Oakmont Village

Legislative Intent:

- 17.1 Street trees are intended to provide shade and screening, and add a graceful component to the streetscape.
- 17.2 When placed in a regular opposite alignment along both sides of streets, street trees are intended to form an Allee effect.

Design Guidelines:

- 17.3 Street trees shall be installed and maintained along both sides of all streets.
- 17.4 One (1) street tree shall be provided for at least every fifty (50) linear feet of street frontage.
- 17.5 Street trees, at the time of planting, shall be no less than three to three and one half inches (3"–3½") in caliper and shall be in accordance with the latest edition of the "American Standard for Nursery Stock," of the American Association of Nurserymen.

Continued on page 17.1



Best Practices Street Trees example along Sidewalk, Mashpee, Mass.



Best Practices Street Tree example, State College, PA

Street Trees

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA

17.6 All street trees shall be one of the following species and/or cultivars, or approved equal, subject to Township approval:

Fraxinus pennsylvanica 'Patmore' – Patmore Green Ash

Gleditsia tricanthos inermis 'Shademaster' – Shademaster Honeylocust

Platanus x acerifolia – London Planetree

Quercus imbricaria – Shingle Oak

Tilia codata – Littleleaf Linden

Ulmus americana 'Valley Forge' - Valley Forge Elm

Zelkova serrata- Japanese Zelkova

Hedges, Fences & Walls

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Best Practices Hedge, Fence, and Wall Combination example, Beaver, PA



Best Practices example of Brick Wall at New Wawa, Thorndale, PA

Legislative Intent:

- 18.1 The Township intends to promote development that provides attractive architectural features in the public realm such as hedges, fences and walls.
- 18.2 Masonry walls and hedges are intended to serve as a type of street wall edge, especially when buildings are not located close to the sidewalk.

Design Guidelines:

- 18.3 Existing walls, fences, and hedges shall be maintained.
- 18.4 Additional walls, fences, and hedges shall be installed and maintained along existing parking areas to strengthen the streetscape character.
- 18.5 Walls shall be compatible with the architectural style of the building on the property.

Continued on page 18.1



Effective Hedge at Eagle and Darby Roads



Effective Hedge along Eagle Road

Hedges, Fences & Walls

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA

- 18.6 Where masonry walls are used, they shall be faced with either stone or brick.
- a. Walls shall have a wall cap, which is at least two (2) inches thick and overhangs the vertical face of the wall by at least one (1) inch.
 - b. Wall caps shall be composed of stone, brick, and/or precast concrete.
 - c. Walls shall be no less than thirty (30) inches in height measured from the ground to the top of the wall.
 - d. Walls faced with brick shall have pilasters spaced at intervals no greater than twelve (12) feet on center.
 - e. Pilasters shall project horizontally no less than two (2) inches beyond the vertical plane of the nominal wall face.
- 18.7 Fences shall be composed of either tubular steel, or aluminum. Vinyl fences and chain link fences are prohibited.
- a. Fences shall be at least thirty (30) to forty-two (42) inches in height measured from the ground to the top of the wall.
 - b. Steel or aluminum fences shall be black or painted black.
- 18.8 Hedges shall be no less than thirty (30) inches in height when installed, and shall be maintained at 30 inches in height.
- a. Shrubs planted to form hedges shall be spaced at intervals so that they will form a continuous visual screen within two (2) years of the date when they are planted.
 - b. Hedges shall be composed of the following shrub species, and/or cultivars, or approved equal:
Euonymus kiautschovicus - Spreading Euonymus
Ilex glabra – Inkberry
Taxus media hatfieldi – Hatfield Yew
Viburnum dentatum - Arrowwood Viburnum

Street Lights

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Street Lights: South Side Works, Pittsburgh, PA



Nightscape, Main Street at Exton: Exton, PA

Legislative Intent:

- 19.1 Pedestrian-scaled street lights are intended to provide an attractive complement to the streetscape.
- 19.2 Street light posts are intended to be equipped to support an attached vertical banner.

Design Guidelines:

- 19.3 Ornamental street lights shall be provided along both sides of all streets.
- 19.4 Street lights shall complement sidewalk and street tree features along the streetscape.
- 19.5 One (1) pedestrian scaled, ornamental street light shall be provided for every ninety (90) linear feet of street frontage.
- 19.6 The Street Light pole shall be 6 inches, fluted, non-tapered aluminum, Sternberg Model# 8714FP6.
- 19.7 The Street Light fixture shall be Elm Street Metal Halide luminare, fixture at 14 foot mounting height, 150 Watts, Sternberg Model# B780R 508 BD/6.



Traditional Street Light along Baltimore Avenue, Media, PA



Sternberg Street Light Detail, Media, PA

Street Lights

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA

- 19.8 Ornamental street lights, when installed, shall be no taller than fourteen (14) feet measured from the mounting surface to the top of the fixture.
- 19.9 Ornamental street lights shall have a matte black finish.

Street Furniture

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Bench in Plaza, Cambridge, MA



DuMor Bench and Victor Stanley Waste Receptacle, Main Street at Exton, Exton, PA

Legislative Intent:

- 20.1 Benches are intended to provide a needed street furnishing for rest and relaxation, and can help to create a more pedestrian friendly streetscape.
- 20.2 Bicycle Racks are intended to send a message: "Bikes are Welcome Here".
- 20.3 Waste Receptacles are intended to provide needed repositories for trash, and can be attractive street furnishings.

Design Guidelines:

- 20.4 One (1) bench shall be installed and maintained along the sidewalk for every block.
- 20.5 DuMor Bench, Model 93, in black, or approved equal, shall be installed and maintained.
- 20.6 Iconic model bicycle racks painted black to match other street furniture shall be utilized.
- 20.7 Bicycle racks shall be installed and maintained in a concrete footing.
- 20.8 Victor Stanley, Model S-42, Ironsites Series Litter Receptacle in black, or approved equal, shall be installed and maintained.



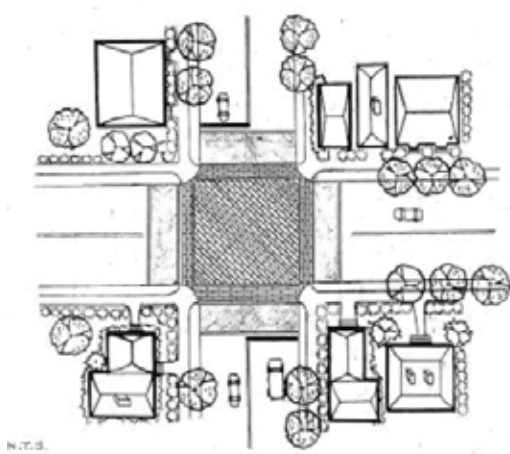
Iconic Bicycle Rack



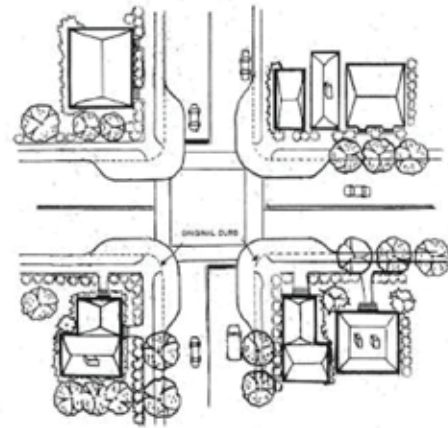
Benches in Sitting Plaza, Eagle Road

Traffic Calming

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Speed Table & Crosswalk at Street Intersection



Curb Bulb Outs

Legislative Intent:

- 21.1 Traffic calming is intended to be a useful measure to help reduce speeding traffic.
- 21.2 Traffic calming is intended to make for a more pedestrian-friendly village setting.
- 21.3 Traffic calming is intended to better enable pedestrian crosswalks across streets.
- 21.4 Raised pedestrian crosswalks at street intersections are intended to provide a useful traffic-calming feature.

Design Guidelines:

- 21.5 Publication No. 383, January 2001, Pennsylvania's Traffic Calming Handbook (TCH) by PennDOT shall be utilized.
- 21.6 Various "Horizontal Deflection" and "Vertical Deflection" measures as per the PennDOT Traffic Calming Handbook shall be utilized.
- 21.7 Buildings, street trees, and landscaping close to the street should be located to create more "friction" to help slow down the traffic.
- 21.8 Raised pedestrian crosswalks should be provided to calm traffic.
- 21.9 Curb bulb-outs should be constructed to reduce the distance for pedestrians to cross the street, and to slow down the traffic and to create larger plazas.



Best Practices example of Speed Table Crosswalk, West Chester, PA



Best Practices example of Raised Unit Paver Crosswalk at Grade, Media, PA

Signage

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Ground Sign, Trader Joes, Media, PA



Blade Sign at Luncheonette, Oakmont Village

Legislative Intent:

- 22.1 Smaller “blade signs” are intended for shops and stores.
- 22.2 Smaller signs are intended for directional orientation.
- 22.3 Wayfinding signage is intended to provide orientation and a sense of place.
- 22.4 Awning Signs are intended to provide a functional and attractive solution to signage.
- 22.5 Ground signs are intended to promote more of a village character.

Design Guidelines:

- 22.6 Well-designed wall signs in scale with the building facade shall be provided.
- 22.7 Smaller blade signs as projecting signs shall be installed and maintained for shops and stores.
- 22.8 Free-standing signs shall extend no higher than twelve (12) feet.
- 22.9 Ground signs, versus free-standing pylon signs, should be installed.
- 22.10 Awning signs shall be provided wherever possible.



Well-Crafted Ground Sign along Eagle Road



Awning Sign, Oakmont Village

Gateways

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Gateway Sign, West Chester, PA



Gateway, Media, PA

Legislative Intent:

- 23.1 Gateways are intended to welcome visitors and provide a positive first impression of the streetscape environment.
- 23.2 Gateways are intended to signify a pedestrian friendly community.

Design Guidelines:

- 23.3 Landscaping, lighting, and signage shall be provided at all gateways.
- 23.4 Gateway Piers should be provided as gateway monuments.
- 23.5 Sidewalks, crosswalks, benches, and bus shelters should be provided as part of gateway design.
- 23.6 Gateway signage shall tie into a cohesive image of the Township.



Gateway Plaza, Oakmont



Gateway Piers, State College, PA

Wayfinding Signage

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Directional Sign



Pole-Mounted Vertical Wayfinding Banner

Legislative Intent:

24.1 Wayfinding signage is intended to aid in navigation for motorists and pedestrians.

Design Guidelines:

24.2 Wayfinding Signage should be installed and maintained in Oakmont Village and along the Eagle Road Corridor.

24.3 Pole-mounted vertical banners shall be installed and maintained at an average interval of 90 feet.



Parking Sign



Library Sign

Bicycle Opportunities

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Bicyclists Need Improved Safety Conditions



White Painted Bicycle Lane

Legislative Intent:

25.1 Bicycle Opportunities are intended to enhance the use and visitation of Oakmont and the Eagle Road Corridor.

Design Guidelines:

- 25.2 Bike paths should be at least 4 feet in width.
- 25.3 Bicycle paths should be well-signed with street signage along and on the street.
- 25.4 One (1) bicycle rack should be provided for every three blocks of street segment.



Bicycle Lane Signage



Iconic Model Bicycle Rack, Main Street at Exton, Exton, PA

Accessory Structures

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Gazebo in Civic Plaza, Manheim, PA



Pavilion, Kentlands, MD

Legislative Intent:

- 26.1 Dumpster enclosures and enclosures for HVAC Units, Utility Boxes, and the like, are intended to become architecturally compatible with the buildings on a site.
- 26.2 Other accessory structures are intended to be architecturally compatible with the overall character of the streetscape.

Design Guidelines:

- 26.3 Enclosed dumpster areas shall be provided with convenient vehicular and pedestrian accessibility.
- 26.4 Decorative wall and fence enclosed dumpster areas, and enclosures for other utilities, shall be installed and maintained.
- 26.5 Kiosks should be provided.
- 26.6 Accessory structures with architectural features similar to the overall streetscape character shall be built, installed, and maintained.

Continued on page 26.1



Attractively Enclosed Dumpster Area, Easton Town Center, OH



Kiosk, State College, PA

Accessory Structures

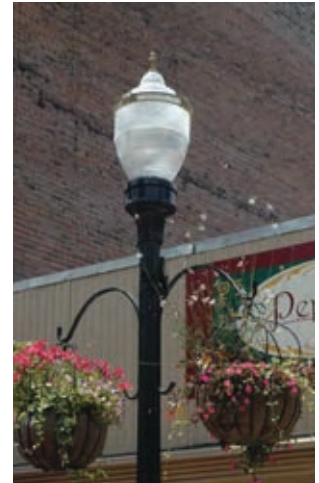
Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Planter, Main Street at Exton, Exton, PA



Planters, South Side Works, Pittsburgh, PA



Hanging Baskets, Butler, PA

Design Guidelines:

- 26.7 Sidewalk Plantings shall be installed and maintained at grade or in containers.
- 26.8 Benches shall be installed and maintained at a rate of at least one(1) per block per side of street.

Design Guidelines:

- 26.9 Hanging Baskets should be installed and maintained in conjunction with street lights.
- 26.10 Waste receptacles shall be installed and maintained at a rate of at least one (1) per block, per side of street.



Bench and Landscaped Area, Wayne, PA



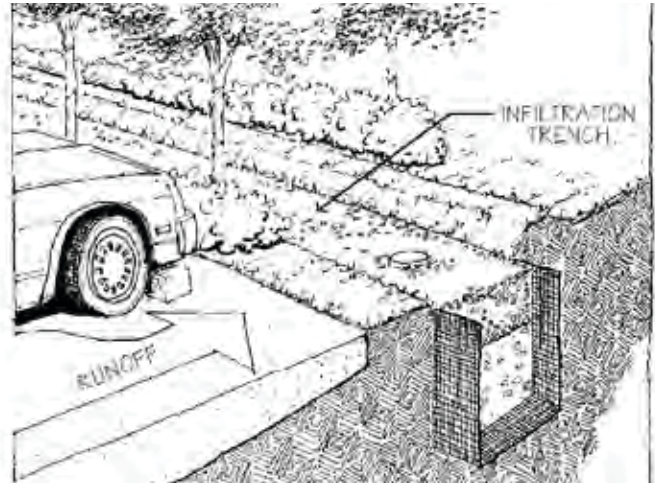
Sidewalk Planter, Beaver, PA

Stormwater Management

Design Guidelines: Oakmont Village & Eagle Road Corridor
Haverford Township, Delaware County, PA



Unit Pavers as Porus Pavement



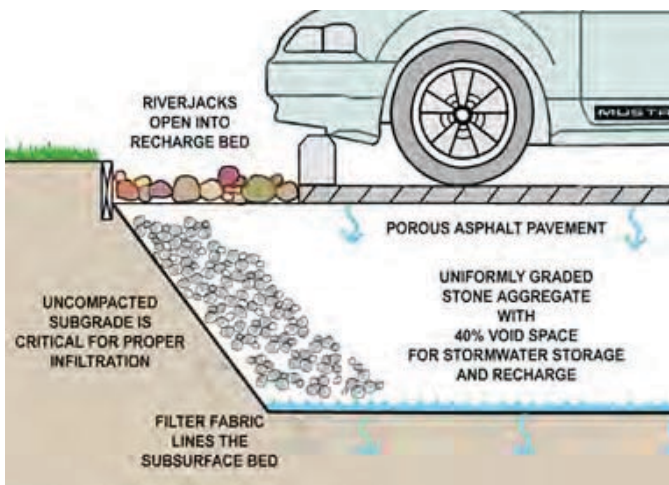
Infiltration Trench Detail

Legislative Intent:

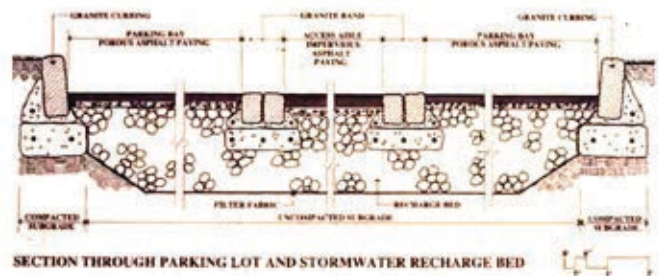
27.1 Stormwater Management is intended to be accommodated in context with the existing conditions in Oakmont Village and the Eagle Road Corridor.

Design Guidelines:

- 27.2 "Light imprint" stormwater management solutions should be constructed and maintained through the use of Infiltrators, Seepage Beds, Porous Asphalt Pavement and other innovative subsurface solutions.
- 27.3 A "storage, swale and filter" solution, versus the conventional "inlet, pipe, pit" solution with rain gardens, bio-retention swales, and the like, should be utilized.



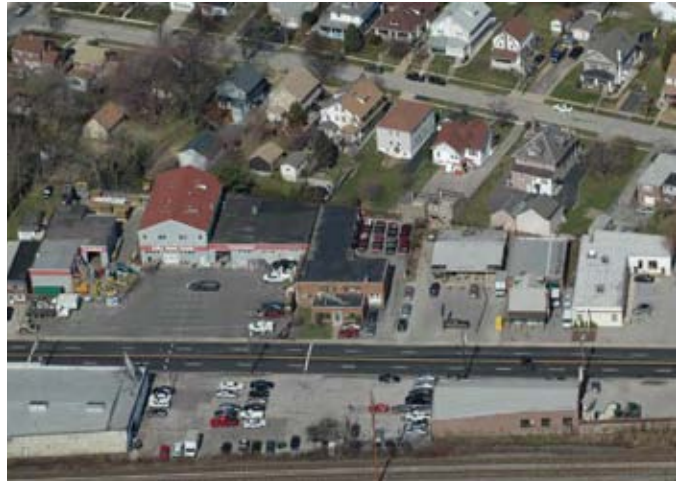
Porous Asphalt Pavement Detail



Stormwater Recharge Seepage Bed Section

Design Guidelines: Haverford Road Corridor

Haverford Township, Delaware County, PA



Comprehensive Plan Addendum

Prepared by:
Thomas Comitta Associates, Inc.
Town Planners & Landscape Architects

October 18, 2007; November 20, 2007;
December 12, 2007; January 4, 2008



Design Guidelines: Haverford Road Corridor

Haverford Township - Delaware County, PA

Outline of Contents:

October 18, 2007; November 20, 2007; December 12, 2007; **January 4, 2008**

1. Purpose & Intent
2. Streetscape & Street Walls
3. Building Location
4. Infill Buildings
5. Off-Street Parking
6. Curbs & Curb Cuts
7. Mixed Use(s)
8. On-Street Parking
9. Sidewalks
10. Crosswalks
11. Pedestrian Gathering Places
12. Street Trees
13. Hedges, Fences & Walls
14. Street Lights
15. Traffic Calming
16. Signage
17. Gateways
18. Bicycle Opportunities
19. Accessory Structures
20. Stormwater Management

Purpose & Intent

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Buildings Located Close to Sidewalks along Haverford Road



Buildings Grouped Close Together, Haverford Road

Legislative Intent:

- 1.1 This Manual is intended to comply with Section 708-A of the Pennsylvania Municipalities Planning Code titled: Manual of Written and Graphic Design Guidelines.
- 1.2 Placemaking, as described and shown in this Manual, is intended to create a more functional and attractive outcome for the quality of life in Haverford Township in general, and along Haverford Road in particular.

Design Guidelines:

- 1.3 This Manual shall be utilized to plan, design, construct and maintain buildings, structures, streetscapes, landscapes and hardscapes.
- 1.4 The places, spaces, buildings, streetscapes, etc. shown and described as "Best Practices" in this Manual shall be emulated.
- 1.5 All land development and redevelopment plan submissions shall be accompanied by Architectural Plans and Building Elevations that are consistent with this Manual.



Best Practices example of Street Trees, Media, PA



Buildings Located Close to Sidewalks along Haverford Road

Streetscape & Street Walls

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Buildings Adjoining Sidewalk Promote Pedestrian Activity of the Streetscape



Small Shops Enhance the Streetscape Environment

Legislative Intent:

- 2.1 The Streetscape is intended to be formed by buildings located close to the sidewalk and/or to be enhanced with such features as street trees, street lights, sidewalks, stoops, and like type amenities.
- 2.2 Street walls are intended to help form and frame the streetscape character along Haverford Road.
- 2.3 Street walls are intended to buffer parking.

Design Guidelines:

- 2.4 The Streetscape shall be embellished with street trees, street lights, sidewalks, fences porches, stoops and other front yard amenities in front of buildings.
- 2.5 Buildings in alignment shall be maintained and/or provided to form Street Walls.
- 2.6 Whenever buildings can not form the Street Wall, the options shown on pages 2.2 through 2.5 and in the photo below, shall be utilized.



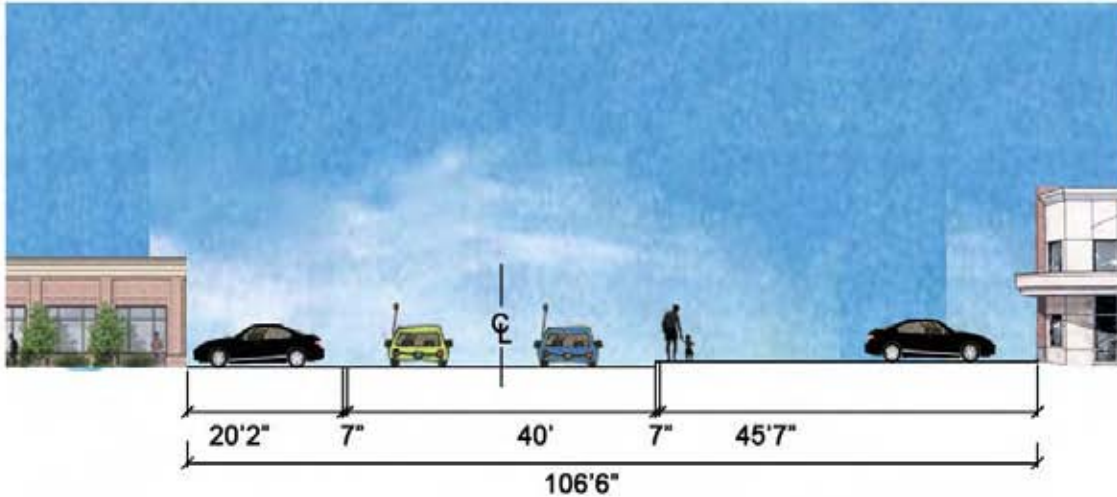
Street Wall Located Close to Sidewalk Screens Off-Street Parking along Eagle Road



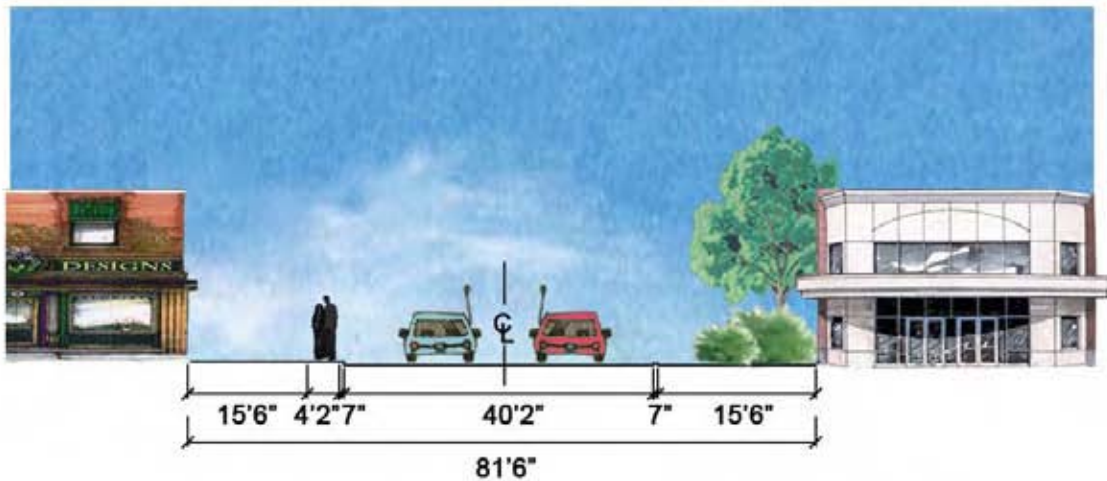
Best Practices example of Street Wall at Corner Parking Lot, West Chester, PA

Streetscape & Street Walls

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Typical Haverford Road Street Section

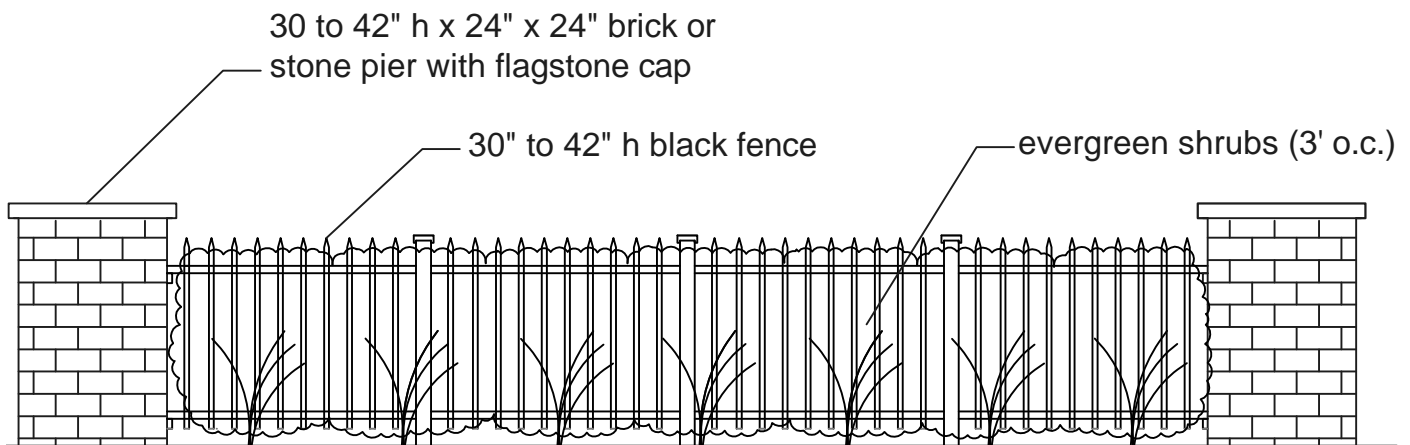


"Cozy" Haverford Road Street Section

Note: Whenever the Street Wall to Street Wall dimension is in the 70 to 85 foot range, a more Village type scale and "Main Street" environment exists.

Streetscape & Street Walls

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



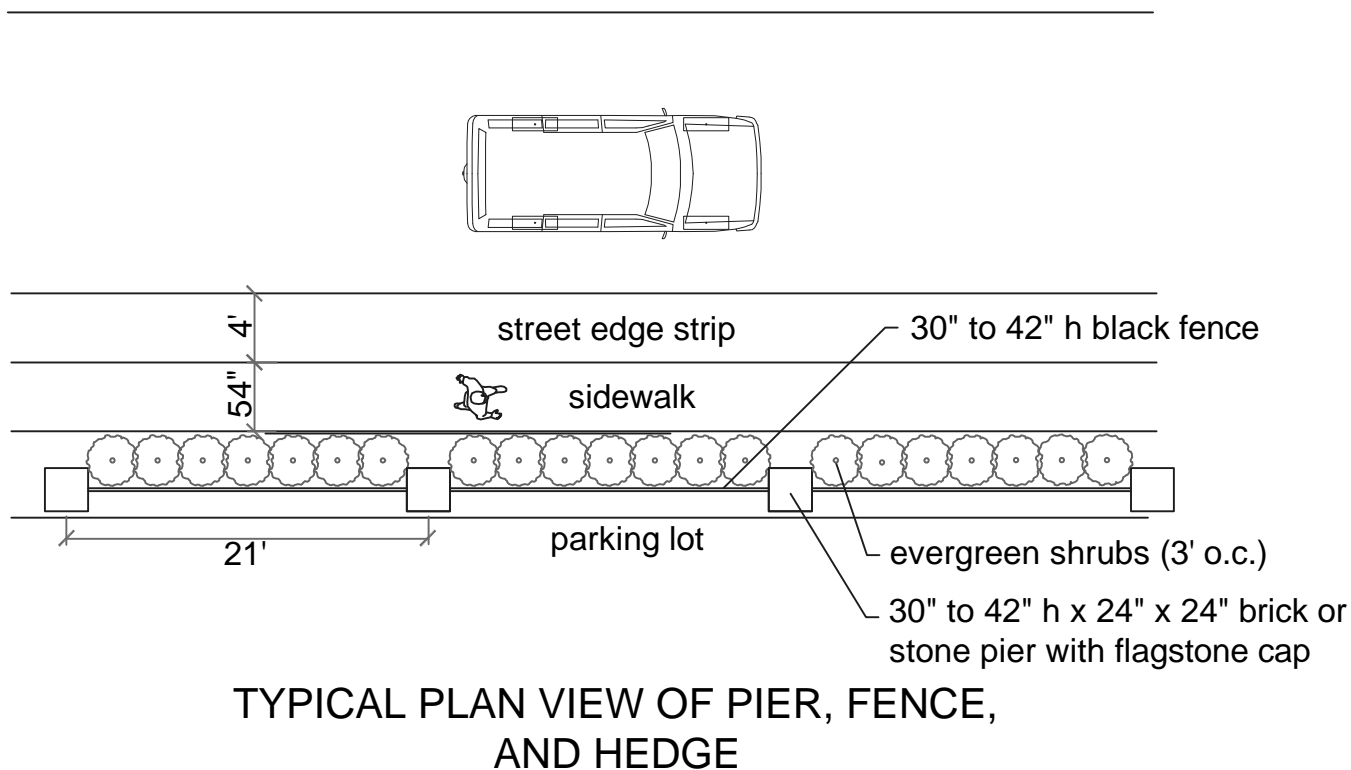
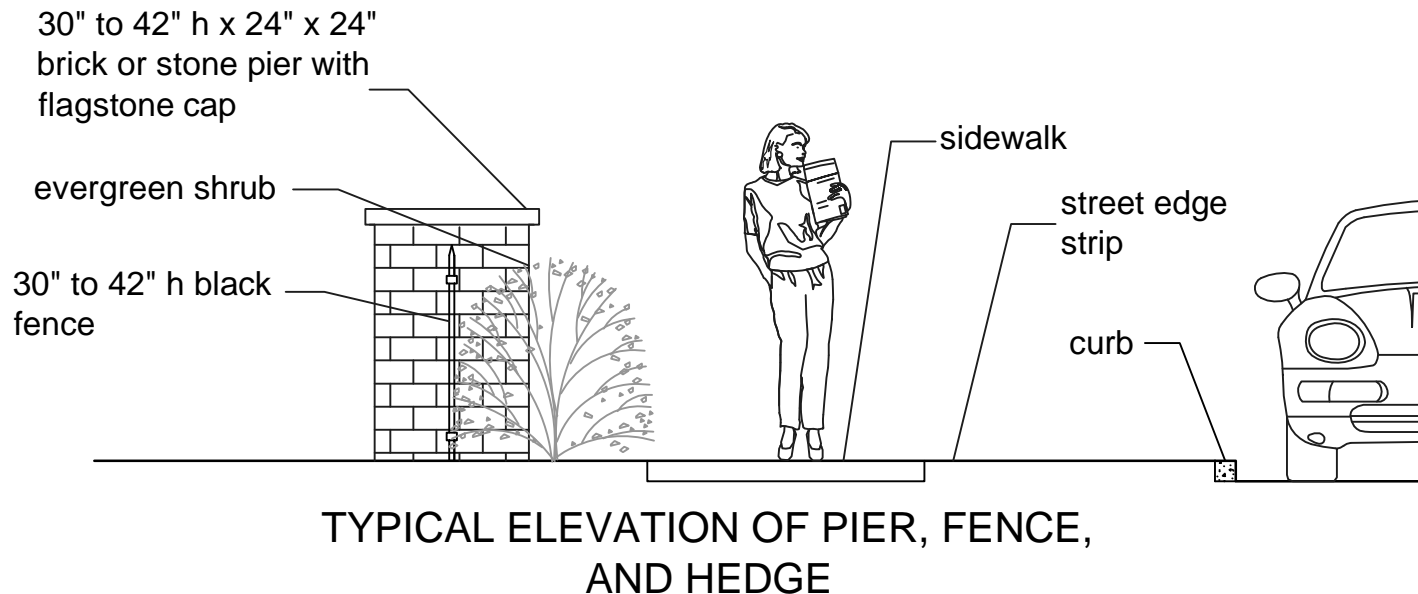
TYPICAL SECTION OF PIER, FENCE,
AND HEDGE



BRICK PIER, FENCE, AND HEDGE AS PARKING LOT
SCREEN

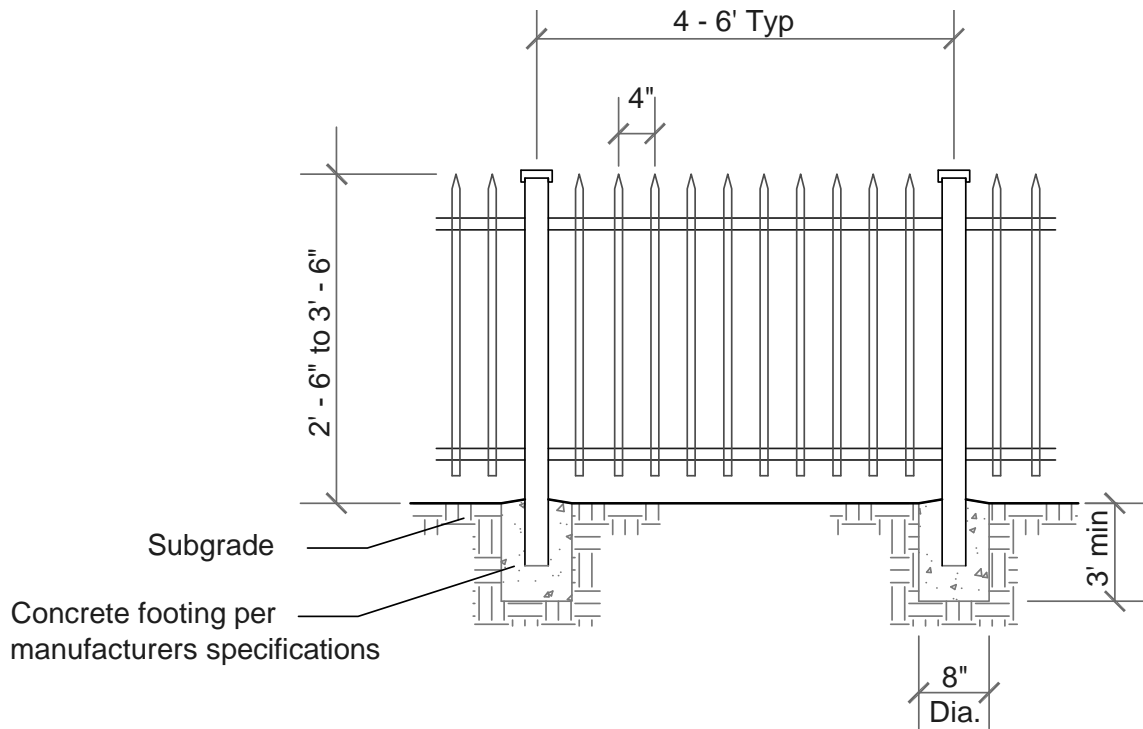
Streetscape & Street Walls

Design Guidelines: Haverford Road Corridor
 Haverford Township, Delaware County, PA

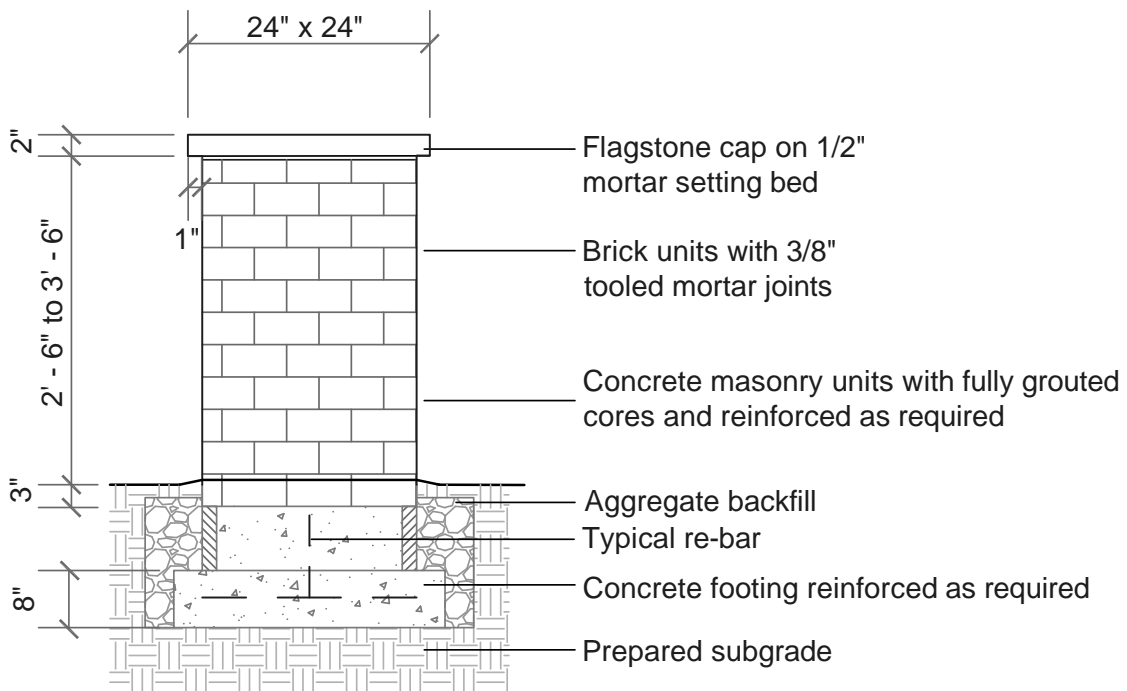


Streetscape & Street Walls

Design Guidelines: Haverford Road Corridor
 Haverford Township, Delaware County, PA



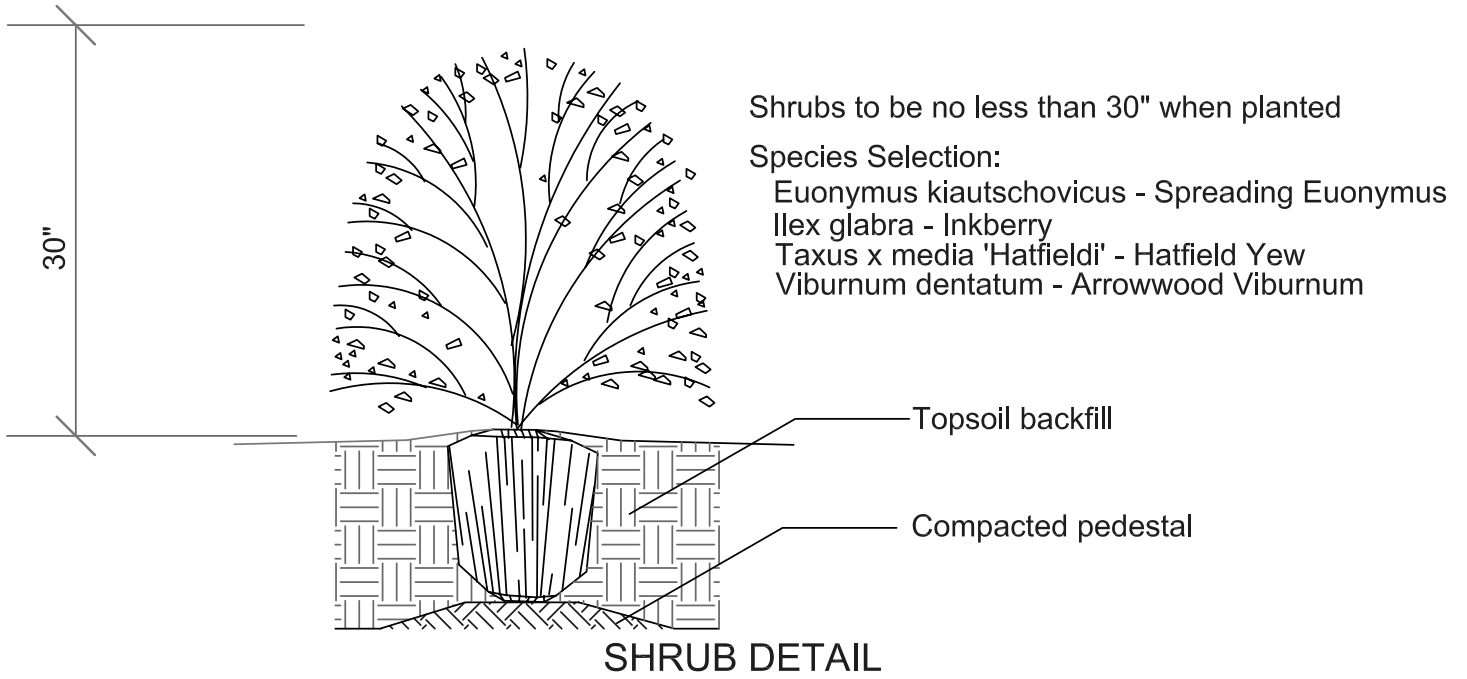
FENCE DETAIL



PIER DETAIL

Streetscape & Street Walls

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Building Location

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Buildings Located Close to Sidewalk Help to Create an “Outdoor Room” Character



Building Forming the Build-To Line

Legislative Intent:

- 3.1 Buildings are intended to be located close to the street to help form the Streetscape.
- 3.2 New buildings are intended to be built and maintained at a “Build-To” Line, in alignment with existing traditional buildings on a block.

Design Guidelines:

- 3.3 New buildings shall be located in line with existing traditional buildings on a block.
- 3.4 New buildings shall be located at a Build-To line measured from the edge of the right-of-way of Haverford Road, or within four (4) feet of the average setback of existing buildings on a block.
- 3.5 If a building cannot be located at a Build-to line, a street wall shall be provided as set forth in the previous Section 2.



Buildings Forming the Build-To Line



Buildings Forming the Build-To Line

Infill Buildings

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



New Two-Story Sovereign Bank, Lantern Hill, Doylestown, PA



CVS Drugstore and Streetscape, Eagle, PA

Legislative Intent:

- 4.1 New buildings are intended to be two-story buildings.
- 4.2 New buildings are intended to promote a streetscape character.

Design Guidelines:

- 4.3 Maintain existing two-story buildings to enhance mixed-use opportunities.
- 4.4 Build infill buildings as two-story structures in context with the Streetscape.
- 4.5 'Anchor' Street Corners with infill buildings.



New Two-Story Dunkin' Donuts, Claymont, DE



Gas Station / Convenience Store, Meredith, NH

Off-Street Parking

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Off-Street Parking Screened with Wall and Trees along Eagle Road



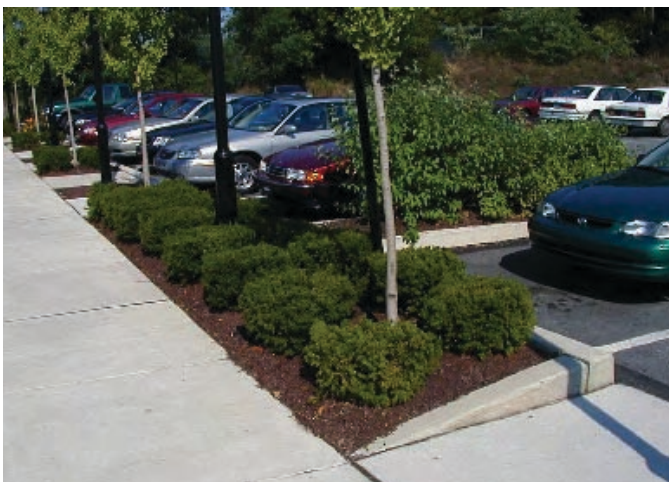
Example of Best Practice for Street Wall of new Wawa in Claymont, Delaware, Buffering Off-street Parking Lot.

Legislative Intent:

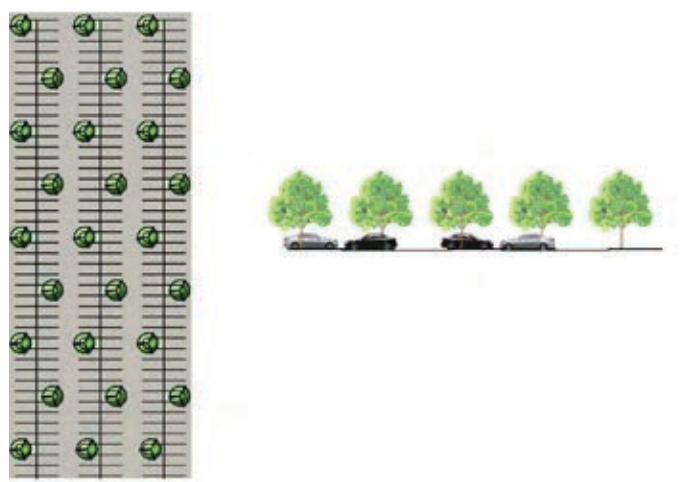
- 5.1 Buildings are intended to screen the parking behind them.
- 5.2 Existing parking lots that are located in front of buildings are intended to be screened with landscaping and/or walls.
- 5.3 Off-street parking lots are intended to be generously landscaped.

Design Guidelines:

- 5.4 New parking shall be located to the rear or side of buildings to the maximum extent possible.
- 5.5 Off-street parking visible from a street shall be screened with walls, fences, hedges and other landscaping at a minimum of 30 inches in height.
- 5.6 Parking lot islands and peninsulas shall be located and landscaped to “break-up” expanses of asphalt, whereby no more than ten (10) parking spaces shall be in a row without being interrupted by a landscaped island.



Best Practice example of Parking Lot with Generous Landscaping, Exton, PA



Best Practice example of Shade Trees in an Off-Street Parking Lot

Curbs & Curb Cuts

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Continuous Curb Without Curb Cut



Continuous Curb Without Curb Cut

Legislative Intent:

- 6.1 Curb cuts are intended to be limited in width, to help control vehicular access and increase safety for pedestrians.
- 6.2 Existing wide curb cuts are intended to be “necked-down” to minimize pedestrian conflicts and to provide space for additional landscaping.

Design Guidelines:

- 6.3 New mid-block curb cuts shall not be created.
- 6.4 Excessively wide curb cuts shall be narrowed to 24 feet where possible.
- 6.5 Existing cross streets and service drives and alleys along the rear of the lot shall be utilized to access parking to the maximum extent possible.
- 6.6 Concrete curbs shall be installed and maintained in accordance with the Haverford Township Subdivision & Land Development Ordinance.



Wide Curb Cuts Should be Narrowed to Provide Increased Safety and Pedestrian Orientation along Haverford Road



Wide Curb Cut Along Haverford Road Increases Vehicular and Pedestrian Conflicts

Mixed Uses

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Apartments Above Commercial, Haverford Road



Apartments Above Commercial along Haverford Road

Legislative Intent:

- 7.1 New Uses are intended be mixed horizontally or vertically.
- 7.2 Live-Work Units are intended to be exceptionally good vertical mixed use buildings.
- 7.3 Mixed Use buildings are intended to provide opportunities for more affordable housing.

Design Guidelines:

- 7.4 Horizontal and vertical mixed-use buildings should be built to the maximum extent possible.
- 7.5 Live-Work Units should be provided to the maximum extent possible.
- 7.6 Offices and/or apartments should also be considered above first floor retail where feasible.



Best Practices example of Ground Floor Cafe, with Apartments Above, Gainesville, FL



Best Practices example of Live-Work Units at Eagleview, Exton, PA

On-Street Parking

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Best Practice example of On-Street Parking along State Street, Media, PA



On-Street Parking Provides a Buffer for Pedestrians along Haverford Road

Legislative Intent:

- 8.1 On-street parking is intended to provide vital overflow parking for patrons and guests.
- 8.2 On-street parking is intended to provide a useful form of traffic calming.
- 8.3 On-Street Parking is intended to provide an “insulation” from vehicular traffic for pedestrians along sidewalks.

Design Guidelines:

- 8.4 On-street parking shall be provided along new streets where feasible.
- 8.5 On-street parking bays shall be 7 feet wide by 22 feet long.
- 8.6 On-street parking shall be utilized as guest parking and overflow parking for guests and patrons.



On-Street Parking is a form of Traffic Calming



Best Practice Example of On-Street Parking from Mashpee, MA

Sidewalks

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Pedestrian Friendly Sidewalks Promote Walkability along Haverford Road



Sidewalk along Haverford Road

Legislative Intent:

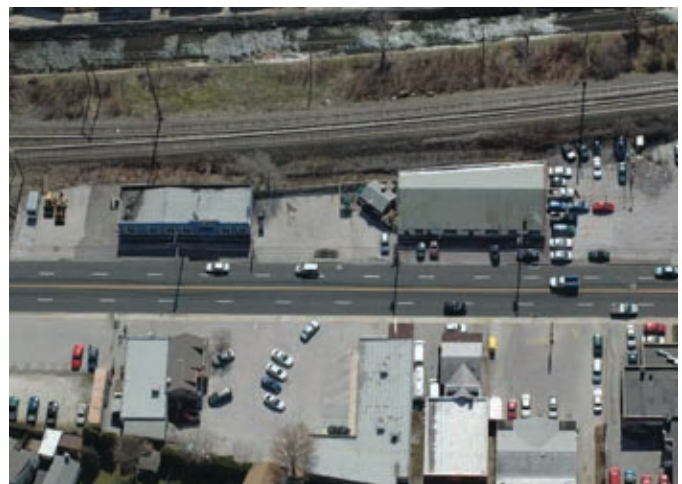
- 9.1 Sidewalks are intended to provide opportunities for continuous pedestrian circulation.
- 9.2 Sidewalks are intended to add a pedestrian friendly feature to the Streetscape.

Design Guidelines:

- 9.2 Sidewalks shall be maintained along both sides of all streets.
- 9.3 All new sidewalks shall be at least 4' - 6' in width (preferably 5'-0" in width) and shall be connected to the buildings on a lot.
- 9.5 All sidewalks shall be constructed of either brick, poured in place concrete, or other material approved by Haverford Township.



Sidewalks with Small Curb Cuts Reduce Pedestrian and Vehicular Conflicts in Media, PA



Sidewalks with Wide Curb Cuts Diminish Pedestrian Presence

Crosswalks

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Well-defined Crosswalks are needed to Continue the Sidewalk System

Legislative Intent:

- 10.1 Crosswalks are intended to continue the sidewalk system in the Township.
- 10.2 Crosswalks in driveway throats (at curb cuts) are also intended to continue the sidewalk system.



Best Practices example of Crosswalk, State Street, Media, PA

Design Guidelines:

- 10.3 Crosswalks shall be at least 6'-0" in width when they cross streets.
- 10.4 Crosswalks located in driveway throats shall be at least 5'-0" in width.
- 10.5 Crosswalks shall be unit pavers such as E.P. Henry Mediterranean interlocking pavers or approved equal.
- 10.6 Where unit pavers can not be installed, zebra stripes shall be painted and maintained.



Best Practice example of Brick Crosswalk in Driveway Throat, West Chester, PA



Well-Defined Crosswalks at Street Intersection, Easton Town Center, OH

Pedestrian Gathering Places

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Peppers Cafe



Civic Plaza, Sewickley, PA

Legislative Intent:

11.1 Pedestrian gathering places and “pedestrian pockets” are intended to provide viable opportunities to celebrate the public realm.

Design Guidelines:

- 11.2 Construct and maintain plazas, pavilions, gazebos, and sitting areas as viable pedestrian pockets.
- 11.3 Install and maintain benches, sitting walls, shade trees, plantings, and lighting in pedestrian areas to enhance attractiveness and safety.



Siting Wall, Newport, RI



Civic Plaza, Cambridge, MA

Street Trees

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Best Practices Street Trees Example along Sidewalk, Mashpee, MA



Street Trees Help Define the Public Realm

Legislative Intent:

- 12.1 Street trees are intended to provide shade and screening, and add a graceful component to the streetscape.
- 12.2 When placed in a regular opposite alignment along both sides of streets, street trees are intended to form an Allee effect.

Design Guidelines:

- 12.3 Street trees shall be installed and maintained along both sides of all streets.
- 12.4 One (1) street tree shall be provided for at least every fifty (50) linear feet of street frontage.
- 12.5 Street trees, at the time of planting, shall be no less than three to three and one half inches (3"–3½") in caliper and shall be in accordance with the latest edition of the "American Standard for Nursery Stock," of the American Association of Nurserymen.

Continued on page 12.1



Best Practices Street Tree example, State College, PA



Street Trees at Wawa along Eagle Road

Street Trees

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA

12.6 All street trees shall be one of the following species and/or cultivars, or approved equal, subject to Township approval:

Fraxinus pennsylvanica 'Patmore' – Patmore Green Ash

Gleditsia tricanthos inermis 'Shademaster' – Shademaster Honeylocust

Platanus x acerifolia – London Planetree

Quercus imbricaria – Shingle Oak

Tilia codata – Littleleaf Linden

Ulmus americana 'Valley Forge' - Valley Forge Elm

Zelkova serrata- Japanese Zelkova

Hedges, Fences & Walls

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Best Practices Hedge, Fence, and Wall Combination example, Beaver, PA



Best Practices example of Brick Wall at New Wawa, Thorndale, PA

Legislative Intent:

- 13.1 The Township intends to promote development that provides attractive architectural features in the public realm such as hedges, fences and walls.
- 13.2 Masonry walls and hedges are intended to serve as a type of street wall edge, especially when buildings are not located close to the sidewalk.

Design Guidelines:

- 13.3 Existing walls, fences, and hedges shall be maintained.
- 13.4 Additional walls, fences, and hedges shall be installed and maintained along existing parking areas to strengthen the streetscape character.
- 13.5 Walls shall be compatible with the architectural style of the building on the property.

Continued on page 13.1



Effective Evergreen Plantings along Haverford Road



Low Street Wall and Hedge, West Chester, PA

Hedges, Fences & Walls

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA

- 13.6 Where masonry walls are used, they shall be faced with either stone or brick.
- a. Walls shall have a wall cap, which is at least two (2) inches thick and overhangs the vertical face of the wall by at least one (1) inch.
 - b. Wall caps shall be composed of stone, brick, and/or precast concrete.
 - c. Walls shall be no less than thirty (30) inches in height measured from the ground to the top of the wall.
 - d. Walls faced with brick shall have pilasters spaced at intervals no greater than twelve (12) feet on center.
 - e. Pilasters shall project horizontally no less than two (2) inches beyond the vertical plane of the nominal wall face.
- 13.7 Fences shall be composed of either tubular steel, or aluminum. Vinyl fences and chain link fences are prohibited.
- a. Fences shall be at least thirty (30) to forty-two (42) inches in height measured from the ground to the top of the wall.
 - b. Steel or aluminum fences shall be black or painted black.
- 13.8 Hedges shall be no less than thirty (30) inches in height when installed, and shall be maintained at 30 inches in height.
- a. Shrubs planted to form hedges shall be spaced at intervals so that they will form a continuous visual screen within two (2) years of the date when they are planted.
 - b. Hedges shall be composed of the following shrub species, and/or cultivars, or approved equal:
Euonymus kiautschovicus - Spreading Euonymus
Ilex glabra – Inkberry
Taxus media hatfieldi – Hatfield Yew
Viburnum dentatum - Arrowwood Viburnum

Street Lights

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Street Lights: South Side Works, Pittsburgh, PA



Nightscape, Main Street at Exton: Exton, PA

Legislative Intent:

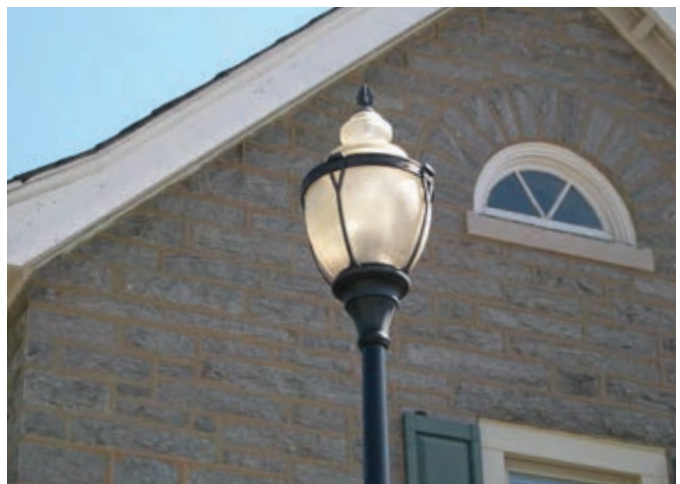
- 14.1 Pedestrian-scaled street lights are intended to provide an attractive complement to the streetscape.
- 14.2 Street light posts are intended to be equipped to support an attached vertical banner.

Design Guidelines:

- 14.3 Ornamental street lights shall be provided along both sides of all streets.
- 14.4 Street lights shall complement sidewalk and street tree features along the streetscape.
- 14.5 One (1) pedestrian scaled, ornamental street light shall be provided for every ninety (90) linear feet of street frontage.
- 14.6 The Street Light pole shall be 6 inches, fluted, non-tapered aluminum, Sternberg Model# 8714FP6.
- 14.7 The Street Light fixture shall be Elm Street Metal Halide luminare, fixture at 14 foot mounting height, 150 Watts, Sternberg Model# B780R 508 BD/6.



Traditional Street Light along Baltimore Avenue, Media, PA



Sternberg Street Light Detail, Media, PA

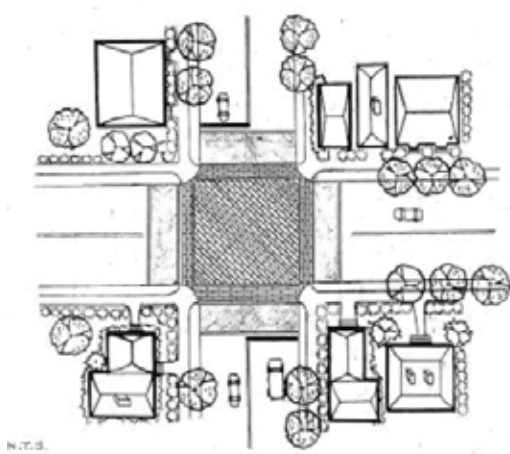
Street Lights

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA

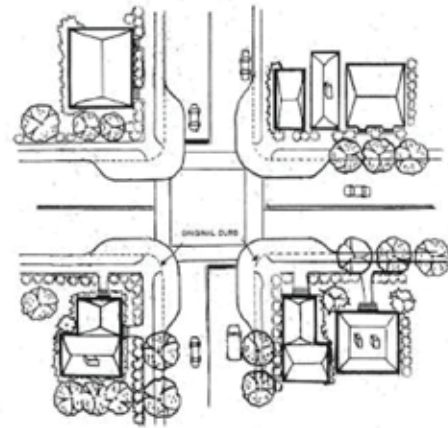
- 14.8 Ornamental street lights, when installed, shall be no taller than fourteen (14) feet measured from the mounting surface to the top of the fixture.
- 14.9 Ornamental street lights shall have a matte black finish.

Traffic Calming

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Speed Table & Crosswalk at Street Intersection



Curb Bulb Outs

Legislative Intent:

- 15.1 Traffic calming is intended to be a useful measure to help reduce speeding traffic.
- 15.2 Traffic calming is intended to make for a more pedestrian-friendly village setting.
- 15.3 Traffic calming is intended to better enable pedestrian crosswalks across streets.
- 15.4 Raised pedestrian crosswalks at street intersections are intended to provide a useful traffic-calming feature.

Design Guidelines:

- 15.5 Publication No. 383, January 2001, Pennsylvania's Traffic Calming Handbook (TCH) by PennDOT shall be utilized.
- 15.6 Various "Horizontal Deflection" and "Vertical Deflection" measures as per the PennDOT Traffic Calming Handbook shall be utilized.
- 15.7 Buildings, street trees, and landscaping close to the street should be located to create more "friction" to help slow down the traffic.
- 15.8 Raised pedestrian crosswalks should be provided to calm traffic.
- 15.9 Curb bulb-outs should be constructed to reduce the distance for pedestrians to cross the street, and to slow down the traffic.



Best Practices example of Speed Table Crosswalk, West Chester, PA



Best Practices example of Raised Unit Paver Crosswalk at Grade, Media, PA

Signage

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Blade Signs along Haverford Road



Blade Sign at Luncheonette, Oakmont Village

Legislative Intent:

- 16.1 Smaller "blade signs" are intended for shops and stores.
- 16.2 Smaller signs are intended for directional orientation.
- 16.3 Awning Signs are intended to provide a functional and attractive solution to signage.
- 16.4 Ground signs are intended to promote more of a village character.

Design Guidelines:

- 16.5 Well-designed wall signs in scale with the building facade shall be provided.
- 16.6 Smaller blade signs as projecting signs shall be installed and maintained for shops, stores and businesses.
- 16.7 Free-standing signs shall extend no higher than twelve (12) feet above grade.
- 16.8 Ground signs, versus free-standing pylon signs, should be installed.
- 16.9 Awning signs should be provided wherever possible.



Ground Sign, Trader Joes, Media, PA



Awning Sign, Oakmont Village

Gateways

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Gateway Plaza, Oakmont



Gateway, Media, PA

Legislative Intent:

- 17.1 Gateways are intended to welcome visitors and provide a positive first impression of the Township.
- 17.2 Gateways are intended to signify a pedestrian friendly community.

Design Guidelines:

- 17.3 Landscaping, lighting, and signage shall be provided at all Gateways.
- 17.4 Gateway Piers should be provided as gateway monuments.
- 17.5 Sidewalks and crosswalks, should be provided as part of gateway design.
- 17.6 Gateway signage shall tie into a cohesive image of the Township.



Gateway Sign, West Chester, PA



Gateway Piers, State College, PA

Bicycle Opportunities

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Bicyclists Need Improved Safety Conditions



White Painted Bicycle Lane

Legislative Intent:

18.1 Bicycle Opportunities are intended to enhance the use of the Haverford Road Corridor.

Design Guidelines:

- 18.2 Bike paths should be at least 4 feet in width.
- 18.3 Bicycle paths should be well-signed with street signs along and on the street.
- 18.4 One (1) bicycle rack should be provided for every three blocks of street segment.



Bicycle Lane Signage



Iconic Model Bicycle Rack, Main Street at Exton, Exton, PA

Accessory Structures

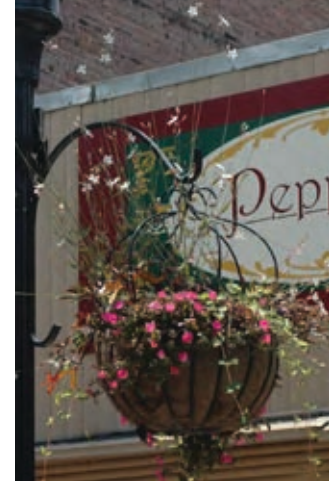
Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Attractively Enclosed Dumpster Area, Easton Town Center, OH



Hanging Baskets, Butler, PA



Legislative Intent:

- 19.1 Dumpster enclosures and enclosures for HVAC Units, Utility Boxes, and the like, are intended to become architecturally compatible with the buildings on a site.
- 19.2 Hanging baskets are intended to beautify the streetscape.

Design Guidelines:

- 19.3 Enclosed dumpster areas shall be provided with convenient vehicular and pedestrian accessibility.
- 19.4 Decorative wall and fence enclosed dumpster areas, and enclosures for other utilities, shall be installed and maintained.
- 19.5 Hanging baskets should be installed and maintained on street light poles.



Decorative Dumpster Enclosure, Strafford, PA



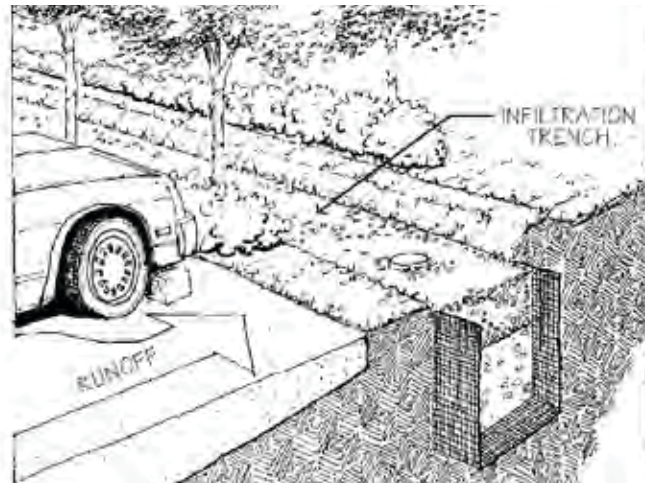
Pedestrian Accessible Dumpster Enclosure with Fence, Springhouse at Thornbury, PA

Stormwater Management

Design Guidelines: Haverford Road Corridor
Haverford Township, Delaware County, PA



Unit Pavers as Porous Pavement



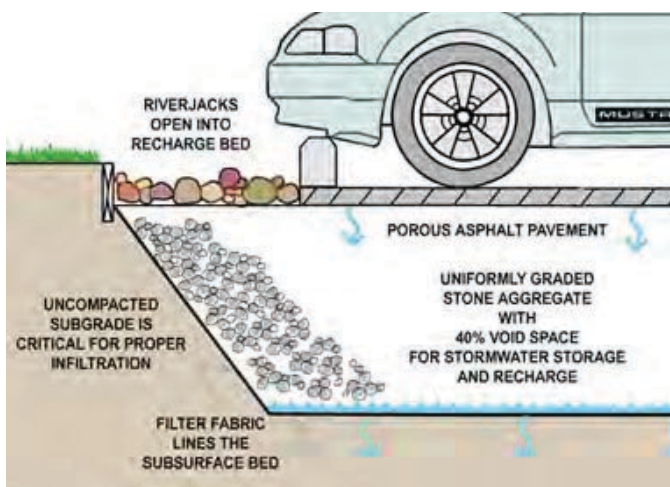
Infiltration Trench Detail

Legislative Intent:

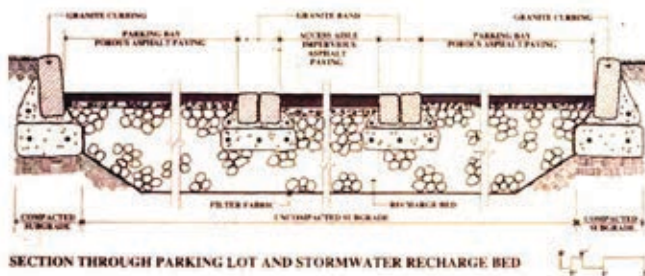
20.1 Stormwater Management is intended to be accommodated in context with the existing conditions in Oakmont Village and the Eagle Road Corridor.

Design Guidelines:

- 20.2 "Light imprint" stormwater management solutions should be constructed and maintained through the use of Infiltrators, Seepage Beds, Porous Asphalt Pavement and other innovative subsurface solutions.
- 20.3 A "storage, swale and filter" solution, versus the conventional "inlet, pipe, pit" solution with rain gardens, bio-retention swales, and the like, should be utilized.



Porous Asphalt Pavement Detail



Stormwater Recharge Seepage Bed Section

Haverford Road Corridor: Goals, Objectives & Strategies

Haverford Township - Delaware County, Pennsylvania

Overarching Goals: To Transform Haverford Road (mid-section) into an attractive and functional streetscape environment. To create a more unified public realm for transportation, service, retail and residential uses, by employing the objectives and strategies set forth below.



Locate Buildings to help Define the Public Realm:

- Locate buildings in alignment with one another to help define the Streetscape

Provide for a Street Wall Effect:

- Locate buildings closer to sidewalks
- Create street walls to help define the Streetscape
- Create street walls to conceal parking lots

Better Manage Off-Street Parking Lots:

- Park on-street if possible
- Promote shared parking & shared parking lots
- Park on the side and to the rear of buildings

Provide for On-Street Parking:

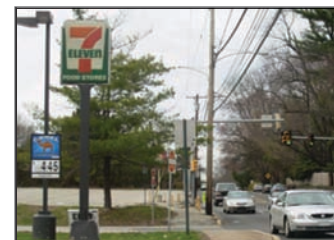
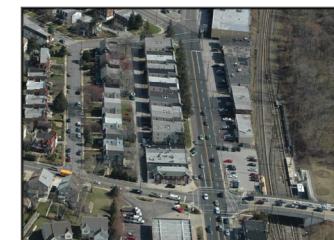
- Park parallel to curb lines wherever possible
- Minimize / reduce curb cuts

Provide for Improved Pedestrian Accessibility:

- Maintain sidewalk networks
- Provide Crosswalks
- Strengthen sidewalk connections to mass transit stations
- Utilize pedestrian linkages along the bus route corridor from Ardmore Junction to Oakmont

Encourage Increased Pedestrian Activity:

- Increase pedestrian activity with second floor office and/or residential uses
- Increase pedestrian activity with restaurant, retail and other commercial uses, as well as attractive light industrial uses



Create and Maintain Gateways:

- Create a TOD Gateway at Ardmore Junction
- Extend the Gateway to Loraine Street
- Create a Gateway at Haverford Road & Eagle Road
- Create a Gateway at Haverford Road & Karakung Drive
- Position Gateways as shown on the following map

Encourage Smaller Footprint Buildings:

- Minimize building sizes, due to small existing lot sizes
- Minimize building sizes, due to existing limited parking opportunities
- Minimize building sizes, due to existing shallow lot depths
- Require two-story buildings to economize on building footprints
- Limit the ground floor area of new buildings to no more than 8,000 square feet

Improve the Appearance of Street Corners:

- Discourage surface parking lots at street corners
- "Anchor" street corners with buildings

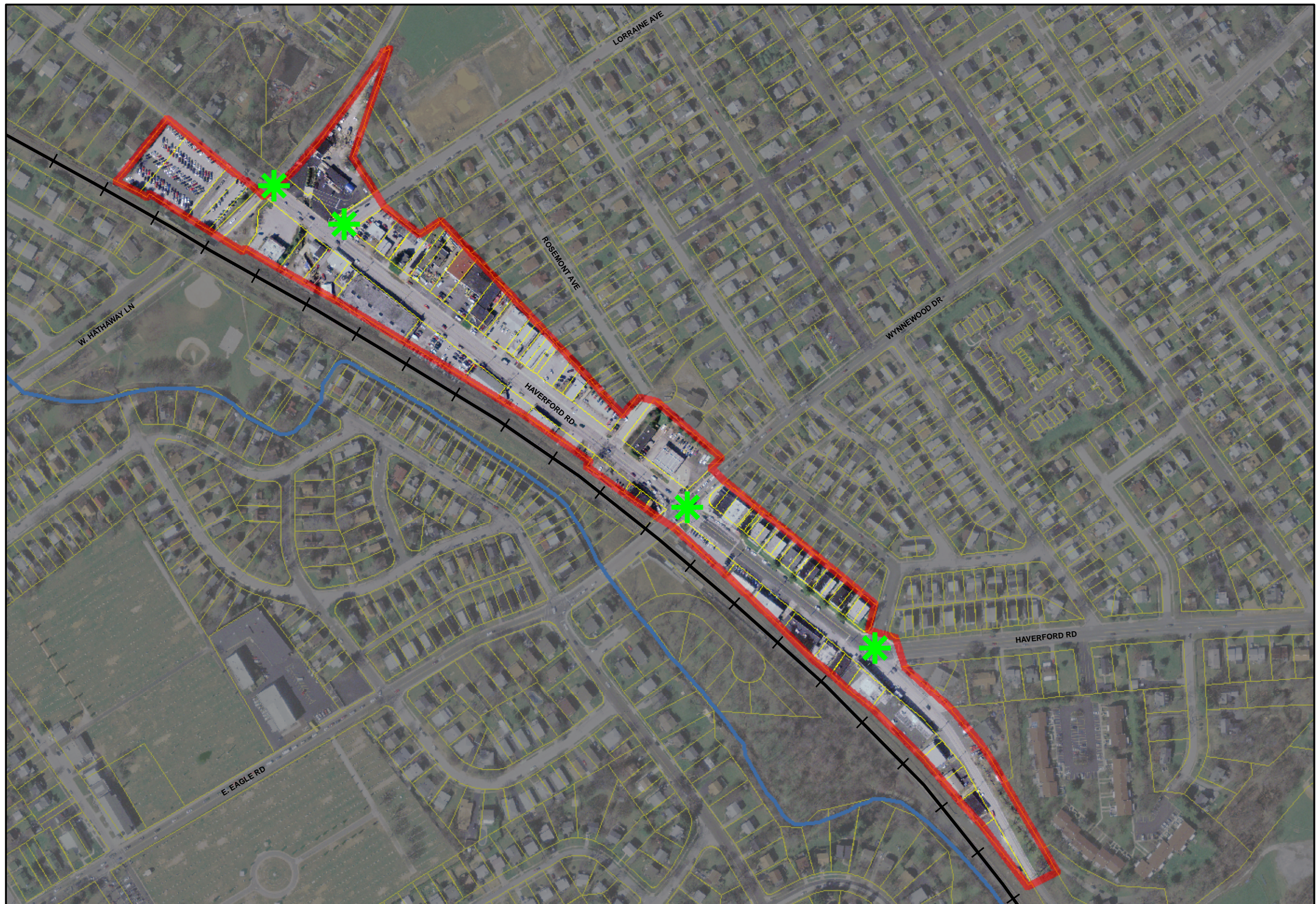
Provide and Maintain Street Trees & Other Landscaping:

- Create a complete "Greenway" with Street Trees
- Plant and maintain hedges

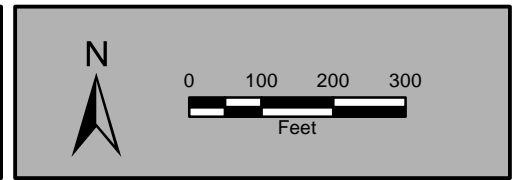
Improve the Quality of Signage:

- Minimize free-standing pylon signs
- Promote ground signs
- Promote well-designed signs

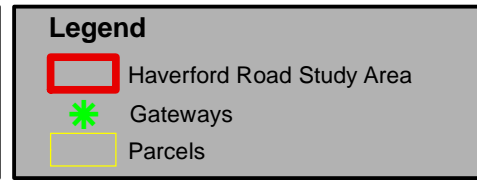




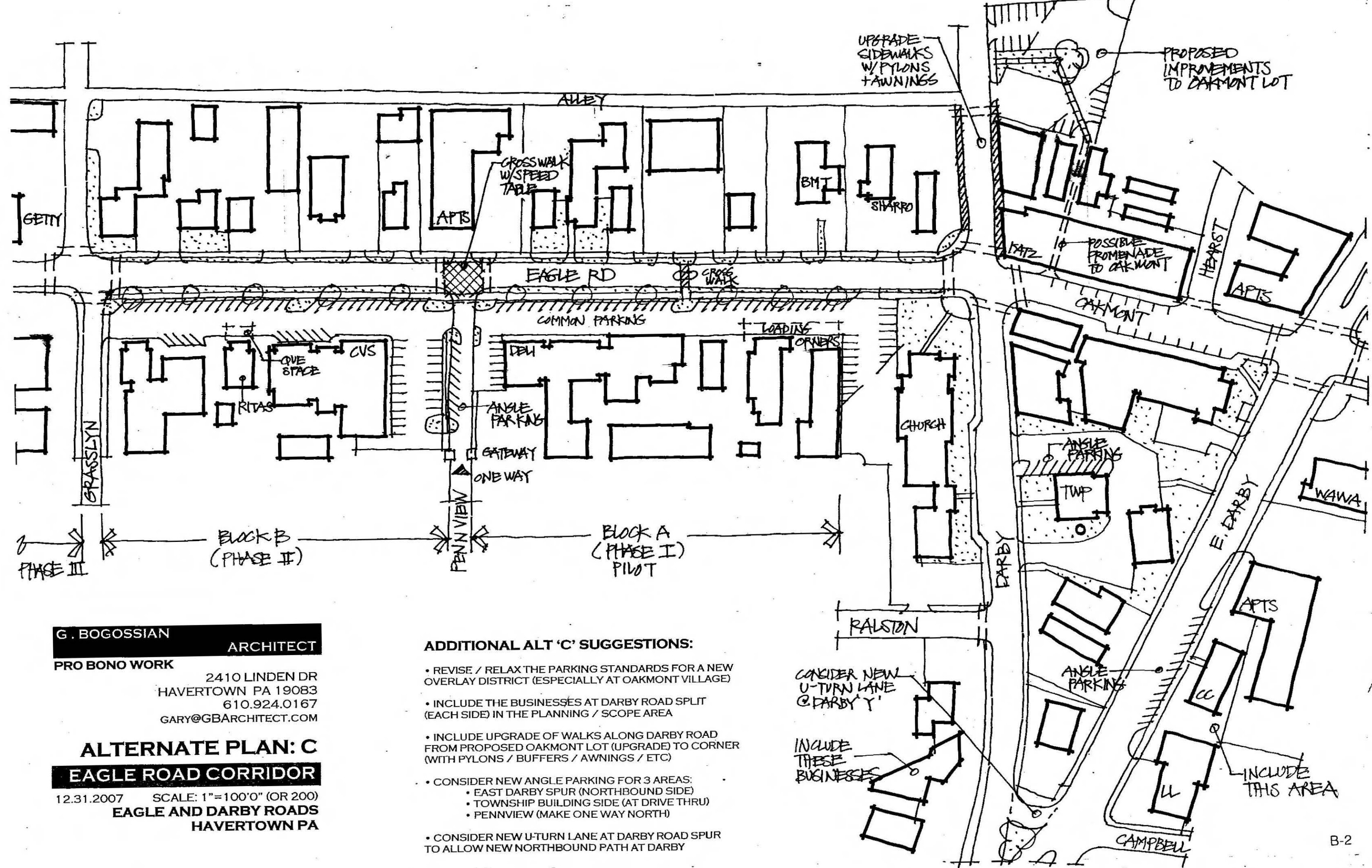
Haverford Road Corridor
 Comprehensive Plan Addendum: Visioning Component
 Haverford Township
 Delaware County, Pennsylvania



Data Source:
 Base Features & 2005 Aerial
 Photos: Delaware County Planning
 Department, May 2007



Thomas Comitta Associates, Inc.
 Town Planners & Landscape Architects
 18 West Chestnut Street
 West Chester, PA 19380
 June 14, 2007; Revised: June 22, 2007;
 June 29, 2007



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ALTERNATE PLAN: C
EAGLE ROAD CORRIDOR
 12.31.2007 SCALE: 1"=100'0" (OR 200)
EAGLE AND DARBY ROADS
HAVERTOWN PA

- ADDITIONAL ALT 'C' SUGGESTIONS:**
- REVISE / RELAX THE PARKING STANDARDS FOR A NEW OVERLAY DISTRICT (ESPECIALLY AT OAKMONT VILLAGE)
 - INCLUDE THE BUSINESSES AT DARBY ROAD SPLIT (EACH SIDE) IN THE PLANNING / SCOPE AREA
 - INCLUDE UPGRADE OF WALKS ALONG DARBY ROAD FROM PROPOSED OAKMONT LOT (UPGRADE) TO CORNER (WITH PYLONS / BUFFERS / AWNINGS / ETC)
 - CONSIDER NEW ANGLE PARKING FOR 3 AREAS:
 - EAST DARBY SPUR (NORTHBOUND SIDE)
 - TOWNSHIP BUILDING SIDE (AT DRIVE THRU)
 - PENNVIEW (MAKE ONE WAY NORTH)
 - CONSIDER NEW U-TURN LANE AT DARBY ROAD SPUR TO ALLOW NEW NORTHBOUND PATH AT DARBY

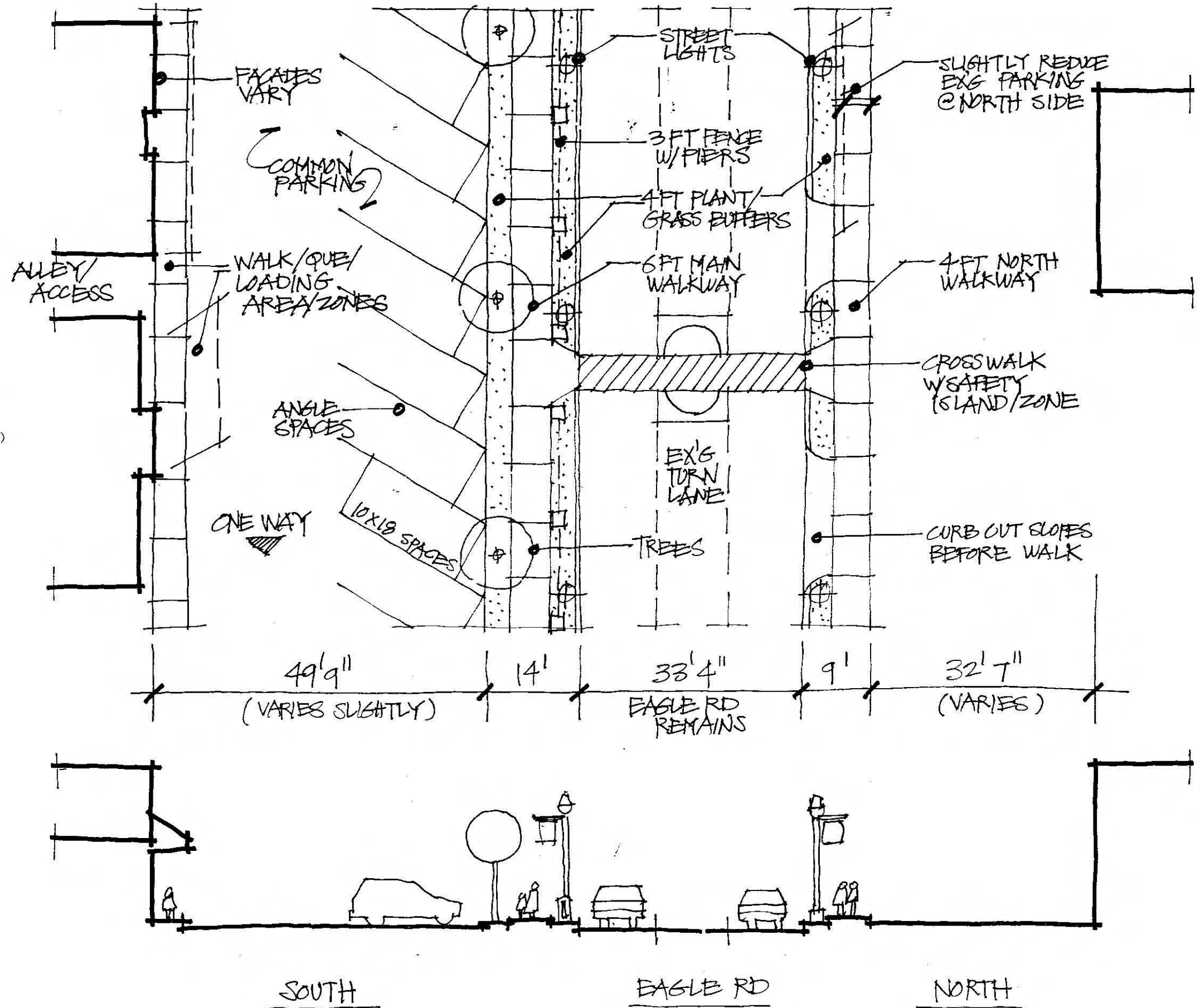
CONSIDER NEW U-TURN LANE @ DARBY Y'

INCLUDE THESE BUSINESSES

INCLUDE THIS AREA

ALTERNATE 'C' FEATURES:

- LEAVES EAGLE ROAD PAVING AREA / DIMENSIONS INTACT (FOR LOWER COST / EASIER PADOT APPROVALS)
- EAGLE ROAD CENTER TURN LANE REMAINS AS IS (TO PRESERVE ACCESS TO NORTHSIDE BUSINESSES)
- 4 FOOT PLANTING BUFFER SEPARATES PEDESTRIANS FROM VEHICLE TRAFFIC ON EACH SIDE OF EAGLE ROAD (PLUS, A 3 FOOT FENCE W/ PIERS SEPARATE SOUTH SIDE)
- MOVING THE SIDEWALK INBOARD 4 FEET ON NORTH SIDE ALLOWS FOR MULTIPLE CURB CUTS WITHOUT SIDEWALK LEVEL INTERRUPTION, AND ONLY SLIGHTLY REDUCES EXISTING PARKING / SIGNAGE AREAS (BY 4 FEET)
- DESIGN ALLOWS FOR EXISTING VARIATIONS OF BUILDING FACADE ALIGNMENTS AND TYPES ON SOUTH SIDE, ALONG WITH ALLEY ACCESS / QUE SPACE / LOADING AREAS
- PROMENADE WALK DESIGN CAN BE BROUGHT ENIRELY DOWN TO GUM FACTORY SITE / ANCHOR IN FUTURE
- PROVIDES A SMOOTHER PEDESTRIAN LINK TO OAKMONT WITH OPPORTUNITY FOR CORNER GATEWAY / POCKET PARK
- CAN BE PHASED (FOR A TRIAL PROJECT & EASIER BUDGETING)
 - I: PENNVIEW TO DARBY ROAD
 - II: GRASSLYN TO PENNVIEW
 - III: GRASSLYN TO GUM FACTORY



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**ALTERNATE PLAN: C
 EAGLE ROAD CORRIDOR**

12.31.2007 SCALE: 1/16"=1'0" (OR 1/32)
 EAGLE AND DARBY ROADS
 HAVERTOWN PA